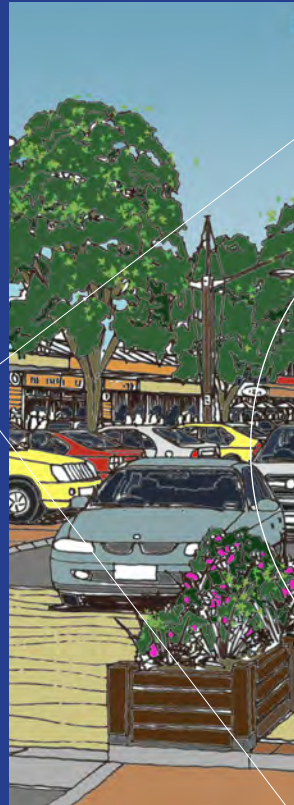




Dalyellup District Centre Outline Development Plan



CERTIFIED THAT DALYELUP SOUTH STRUCTURE PLAN

WAS ADOPTED BY

RESOLUTION OF THE WESTERN AUSTRALIAN PLANNING

COMMISSION ON

Signed for and on behalf of the Western Australian Planning Commission

.....

An officer of the Commission duly authorised by the Commission pursuant to section 24 of the
Planning and Development Act 2005 for that purpose, in the presence of:

..... Witness

..... Date

AND BY

RESOLUTION OF THE COUNCIL OF THE SHIRE OF

CAPEL ON

.....

Chief Executive Officer, Shire of Capel

..... Date

DOCUMENT QUALITY CONTROL

Report prepared by
TME, Chappell Lambert Everett (with Malcom Mackay),
Cossill & Webley & Sinclair Knight Merz
on behalf of
Satterley Property Group

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TME Town Planning Management Engineering

PO BOX 733, BUNBURY

PH: (08) 9791 4411



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INTRODUCTION

The Dalyellup Centre Outline Development Plan (centre plan) has been prepared to guide the future development of the Dalyellup District Centre which will be the major activity centre serving the new community of Dalyellup and surrounding areas.

The vision for Dalyellup District Centre is:

Dalyellup District Centre will be an attractive, diverse, safe and sustainable activity centre with a unique urban village character and will provide a focus for shopping, business, community and recreation activities for the use and enjoyment of the local community and wider district.

The aim is to create a vibrant and sustainable District Centre which will provide a shopping, commercial, social, cultural and civic focus for the Dalyellup community.

The centre plan has been prepared in accordance with the requirements of the Shire of Capel Town Planning Scheme No. 7 (TPS7) which requires the preparation of an outline development plan for the precinct prior to subdivision and development (Appendix 16(2)(c)). The plan has been prepared in accordance with the procedures for the preparation and adoption of outline development plans as set out in clause 5.10 of TPS7.

The centre plan is in two parts.

Part 1 contains the statutory provisions to assist the Council and state government agencies in assessing statutory applications and ensuring the orderly development of the District Centre. The centre plan is attached as *Figure 1*. There are five precincts within the centre plan area, each characterised by location, land use and built form. Objectives, development standards and land use permissibility are specified for each precinct.

Part 2 is the explanatory report which describes the vision, principles and proposals for the District Centre. The concept plan which supports the centre plan is attached at *Figure 2*.



PART ONE: STATUTORY PROVISIONS



1.1 Application

- 1.1.1 This plan applies to the Dalyellup District Centre plan area, being Part of Lot 4550 and 9018, Dalyellup, consisting of all the land contained within the inner-line denoting the centre plan boundary on the centre plan map (*Figure 1*).
- 1.1.2 This plan constitutes an Outline Development Plan for the purposes of clause 5.10 and Appendix 16(2)(c) of the Shire of Capel TPS7.
- 1.1.3 The centre plan area is divided into precincts and the location and boundaries of the precincts are shown on the centre plan map.
- 1.1.4 Unless otherwise specified in this centre plan, the words and expressions used in the centre plan shall have the respective meanings given to them in the Shire of Capel TPS7.
- 1.1.5 Subdivision and development is to be generally in accordance with the centre plan.

1.2 Subdivision, Development and Implementation

- 1.2.1 This part prescribes the objectives, standards, requirements and prerequisites for subdivision and development in the centre plan area.
- 1.2.2 Section 1.3 prescribes the general provisions which apply throughout the centre plan area.
- 1.2.3 Sections 1.4 -1.8 prescribe the provisions which apply to the corresponding precincts designated on the centre plan map.
- 1.2.4 Where any variation arises between a provision of the centre plan and a provision of the scheme, then the provision of the centre plan is to prevail to the extent of that variation and is to apply as an intended variation to the scheme for the purposes of clause 8.10.1.
- 1.2.5 Subject to clause 5.1.2 of the scheme, development approval is to be obtained prior to the commencement of development.
- 1.2.6 In addition to the general development requirements of the scheme, Council shall have regard to the general and specific planning requirements set out in sections 1.3 - 1.8 in granting planning consent.

1.2.7 The Council may require the preparation of a detailed area plan for a precinct prior to the approval of subdivision and development of part of the precinct to demonstrate in more detail how the general provisions and precinct provisions are to be implemented. The Council is to consider and adopt such plans in accordance with clause 5.10.9(b) of the scheme.

1.2.8 A development application may be submitted and approved for all or part of a precinct. Where development is proposed for part of a precinct, the development application is to:-

- a) be consistent with any operative detailed area plan for the precinct; or
- b) show how the proposed development addresses the site context, and the general and precinct-specific objectives and standards.

Where a detailed area plan is prepared for a precinct or a development application is submitted in the absence of an operative detailed area plan, in addition to the provisions set out in sections 1.3 to 1.8, consideration shall be given to the principles conveyed pictorially on the District Centre Concept plan, the Main Street Concept Plan and the explanations of the plans and precincts contained in section 2.9 and 2.10.

1.2.9 The Council may, at its discretion, recommend subdivision or approve development at variance to the precinct boundaries providing the variation is minor and consistent with the general centre plan objectives and standards, and the relevant precinct-specific objectives and standards.

1.2.10 In order to facilitate and co-ordinate the development of the district centre, more detailed plans and strategies may be required to guide implementation and ongoing management. These plans will include, but will not be limited to, a place management plan, landscape strategy, sustainability outcomes and implementation plan and signage strategy. The plans and strategies are to be adopted by the Council and when adopted, will form part of the centre plan and development of the district centre is to comply with them.

1.2.11 Upon completion of each stage of development of the district centre the developer shall submit to the Council an audit report on the incorporation into development designs and their completion of features, equipment and infrastructure used to meet the objectives of the sustainability outcomes and implementation plan.

1.2.12 The first stage of the development of the centre plan area is to include provision of a supermarket and construction of the Main Street.



1.3 General Centre Provisions

1.3.1 The following objectives, requirements and standards apply to all the centre plan area as shown on the centre plan map.

1.3.2 General Centre Objectives

1.3.2.1 Land Use:

- a) Facilitate a mix of uses including shopping, offices, showrooms, community and other complementary uses, creating a genuine community hub and encouraging multi-purpose trips.
- b) Allow for business, showrooms and similar uses to be located within the centre and outside the main retail core accessible to the arterial road network.
- c) Encourage a diversity of housing types and tenure within the centre including a range of affordable housing.
- d) Identify appropriate locations for live and work dwellings and home-based businesses, and ensure that detailed design and planning provisions can encourage their use for work places.
- e) Manage the interface between residential and non-residential land uses in a manner that recognises the role of the centre, as well as the amenity of residential uses.

1.3.2.2 Structural Elements and Networks:

- a) Provide a highly connected street and path network which encourages pedestrian connectivity to, and through, the centre, provides for cyclists (including bicycle parking and end of trip facilities as recommended in the Traffic and Transport Report at Appendix 2) and disbursts vehicle trips. Provide regular and safe road crossing point for pedestrians.
- b) Encourage road design standards which deliver main street principles.
- c) Manage crossover locations to provide safe access.
- d) Design public roads in a way that appropriately manages traffic behavior and speed in areas of high pedestrian movement.
- e) Provide parking areas in locations that allow shared use, co-ordinated access and strong pedestrian connectivity to buildings and nearby streets, whilst minimising their visual impact on the streetscape. Maximise the provision of on-street car parking.

- f) Facilitate the reciprocal use of car parking bays to encourage a high turnover of parking bays and reduce the need for expansive car parking areas.
- g) Allow for the interim use of vacant sites for parking prior to later development to support the initial stages of development.

1.3.2.3 Built Form:

- a) Design buildings to address street frontages and public spaces, and maximise opportunities for passive surveillance from all levels through the placement of entrances and a high degree of street front glazing.
- b) Ensure a high quality of design and encourage the continuity of building frontages along the main street, and on Norton Promenade and Parade Road South, the principle gateways to the centre.
- c) Provide opportunities to integrate signage of an appropriate scale and character, with buildings.
- d) Design residential buildings that are potentially affected by traffic noise from Bussell Highway to respond to the outcomes of the "Dalyellup East Residential Estate - Road Traffic Noise Assessment (May 2011)" and apply to relevant lots suitably worded notifications advising of possible amenity impacts where necessary.

1.3.2.4 Streetscape and Landscape:

- a) Ensure a high quality and attractive public realm with an appropriate range and distribution of public spaces, and a street environment which provides a unique sense of space and local identity and minimises the use of off-site water supplies.
- b) Establish local landmarks, artwork and landscape elements as an aid to legibility.
- c) Encourage the use of street trees in appropriate locations within the public realm.
- d) Recognise Crime Prevention Through Environmental Design principles as a tool to create a safe and enjoyable pedestrian experience.

1.3.2.5 Sustainability:

- a) Consider solar passive design principles in lot orientation and the detailed design of buildings.
- b) Promote energy efficient and water efficient building practices.
- c) Promote the use of drought tolerant and low maintenance plants and vegetation.



- d) Use deciduous street trees where possible to provide shade from the summer sun and sun penetration during winter.
- e) Recognise that development of the centre will occur in a staged manner and that the ultimate land use and development outcomes will occur as the centre evolves.
- f) Create a structure and land use control regime which will facilitate land use change over time.
- g) Meet the requirements of the WAPC guidelines for Better Urban Water Management through the preparation and implementation of relevant levels of water management strategy for the subdivision and development of the land and in particular to comply with the Dalyellup East Water Management Plan.

1.3.2.6 Community:

- a) Encourage and facilitate the role of the District Centre as a focus for community activity and development.
- b) Provide accessibility for all ages and physical ability.
- c) Acknowledge the presence of youth and the need to accommodate their formal and informal recreational pursuits.
- d) Provide for safe, well maintained meeting places with appropriate infrastructure to facilitate community activities.
- e) Provide for places and spaces for seasonal decorations and display.
- f) Provide for produce and craft markets to occur in either public or private spaces.
- g) Provide for adequate community security through the preparation of a Community Security Plan having regard to the WAPC "Designing Out Crime Planning Guidelines".

1.3.3 Information to be provided in all applications for Planning Consent

In addition to the information required under Part 8 of the Scheme, all development applications for non-residential development are to include the following information:

- a) Setbacks to lot boundaries.
- b) Proposed land uses and floorspace areas.
- c) Number, layout and dimension of all car parking spaces on site.
- d) Location of on street car parking.

- e) Supporting information to allow Council to consider any variation proposed to car parking standards.
- f) Road network, vehicular access locations, paths and cycle ways, generally in accordance with the Traffic and Transport Report and as shown on the centre plan.
- g) Connections to, and locations of, pedestrian crossing points to public roads.
- h) Indicative site levels.
- i) Location of public and private open space.
- j) Proposed landscape treatments.
- k) Treatment of landmark sites.
- l) Provision for streetfront glazing, entrances and awnings.
- m) Location of services, loading / unloading areas, storage, drainage and rubbish collection areas.
- n) Location of signage and fencing.
- o) Building design features that meet the general and precinct-specific planning requirements.
- p) Building and site development features, equipment and infrastructure to be used to meet objectives of a sustainability outcomes and implementation plan.

1.3.4 General Development Standards

In addition to the relevant precinct-specific development standards listed under sections 1.4 –1.8, the following general development standards apply to all development within the District Centre:

- a) Car parking is to be provided in accordance with the provisions of the scheme, except where otherwise provided for under this part. On street car parking immediately adjoining the site can be included in calculating car parking provision.
- b) Variations to the car parking standards allowing for lesser or requiring greater numbers of parking spaces than specified under the scheme, or this part, for non-residential development will be considered as part of the assessment of the development application where:
 - i) development generally has an active presentation to the public realm;



- ii) car parking is located in accordance with the precinct-specific development standards; and
 - iii) nearby on-street car parking or reciprocal use of off-street parking is available to meet the parking requirements.
 - iv) the particular format of floor space is likely to generate a level of demand for parking spaces which is generally known to be lower or higher than is required by the scheme.
- c) Vehicular crossovers to lots adjoining the Main Street and Norton Promenade should be located in accordance with the preferred vehicular crossover locations on the centre plan. Variations to these locations and/or additional suitable connections can be considered as part of a development application with supporting justification.
- d) Buildings and structures on landmark sites should be designed in a manner that recognises the strategic location of the site. This may include one or more of the following: architectural features; glazing; public art; landscape features; materials; colour; height / scale; major openings; balconies and other applied structures.
- e) Service areas should be predominately screened from public view
- f) All off street parking areas are to be landscaped with sufficient space for trees and plants to grow and thrive.
- g) Provision shall be made for convenient and effective use of a range of transport modes including pedestrian, cyclists and public transport.

1.3.5 Land Use

1.3.5.1 Sections 1.4 –1.8 set out the permitted “P” land uses in accordance with clause 3.2.2 of the scheme for the various precincts.

1.3.5.2 The permissibility of other land uses is at the discretion of Council. Land uses are to be considered “AA” in accordance with clause 3.2 of the scheme where they are consistent with the precinct-specific objectives and land use provisions under sections 1.4 –1.8.

1.3.6 Staging and Specific Actions Required of the Subdivider

1.3.6.1 Prior to the subdivision of the centre plan area the owner of the land shall submit for approval by the Council a staging strategy to show:

- a) The proposed stages of subdivision and development of the subdivided land.

- b) Arrangements for and timing of provision of utility services to the various precincts of the centre plan, construction of the road network and intersections, development of public open space and drainage areas, earthworks and fill, and the stabilization and landscaping of undeveloped portions of the centre plan area during interim stages.
- c) Proposals for the staged construction of Parade Road (south) depicted on the Dalyellup East Local Structure Plan including any land exchanges to offset the loss of land by the Riding for The Disabled Association of WA South West Group Incorporated (RDA).
- d) Arrangements for and timing of the implementation of the Wetland Management Plan for the Wetland UFI 13943 west of Parade Road.
- e) How the intent of the general and precinct specific provisions of the Outline Development Plan will be achieved.
- f) The method of binding the subdivider or future owners to meet obligations arising from the staging strategy.
- g) The proposed timing and responsibility for the preparation of the various plans and strategies required by the ODP including:
 - Place Management Plan
 - Landscape Strategy
 - Sustainability Outcomes and Implementation Plan
 - Signage Strategy
 - Water Management Strategy at subdivision and development scales
 - Community Security Plan
 - Other detailed management plans such as parking management and maintenance as may be required.

1.3.6.2 The Community Purpose site shall be transferred to the Shire of Capel in a fully serviced condition within 12 months of the Shire giving notice that it is required in order to provide services to the community of Dalyellup.



1.4 Planning Requirements for Precinct A – Retail Core

1.4.1 Precinct A is the main street retail core, where the emphasis will be on maximising activity and intensity along the main street and the public realm.

1.4.2 The following provisions apply to the area shown as “Precinct A” on the centre plan, in addition to the provisions of section 1.3.

1.4.3 Objectives

- a) Create a pedestrian friendly main street retail core with high amenity and a strong sense of place.
- b) Encourage a mix of compatible retail and other intensive uses including uses which generate activity outside core business hours.
- c) Encourage richness in the streetscape, including articulation of buildings, windows and openings to create visual interest at street level.
- d) Facilitate strong and direct pedestrian connectivity to, along and across the main street.
- e) Encourage a continuous paved and covered pedestrian walkway along the edge (verge) of the main street.
- f) Provide intimate community meeting places in public areas with seating and other amenities.
- g) Allow on street parking where possible. Locate off-street parking behind buildings fronting the main street.
- h) Provide the opportunity for temporary activities in the street, such as alfresco dining, external display and public events to create interest.

1.4.4 Land Use

1.4.4.1 The following land uses are “P” uses in Precinct A:

- a) Shop (including supermarket and department store)
- b) Restaurant
- c) Office
- d) Take Away Food Outlet (excluding drive through fast foods)
- e) Liquor Store
- f) Tavern
- g) Betting Agency

- h) Art Gallery
- i) Art and Craft Studio and Sales
- j) Public Amusement
- k) Amusement Parlour
- l) Consulting Rooms
- m) Medical Centre
- n) Health Studio
- o) Car Park
- p) Multiple Dwelling
- q) Market

1.4.4.2 Other appropriate uses may be considered as “AA” uses at Council’s discretion where the use is consistent with the objectives of the precinct, and promotes the shopping and main street role of the precinct.

1.4.4.3 Single and grouped dwellings are not permitted within Precinct A.

1.4.4.4 Multiple dwellings are not permitted on the ground floor of buildings fronting the main street.

1.4.5 Development Standards

1.4.5.1 A nil setback generally applies to the ground floor front façade of buildings adjoining the main street, except where necessary to provide a forecourt, building articulation, alfresco dining, or other feature that adds amenity and interest to the streetscape.

1.4.5.2 Buildings adjoining the main street should generally have a continuous frontage, except where required for vehicular access, servicing or to provide articulation and interest to the streetscape.

1.4.5.3 A covered, and generally continuous, pedestrian walkway should be provided to the front façades of building adjoining the main street as part of the road reserve. The awning should be designed at a pedestrian scale and provide an acceptable degree of shade and shelter.

1.4.5.4 Town squares, open spaces and principal pedestrian walkways, should be provided generally in accordance with the centre plan.

1.4.5.5 On site car parking should not be located between the front building



setback line and the main street. Large areas of car parking should be located behind buildings fronting the main street and Norton Promenade, as identified on the centre plan as preferred centre core car parking locations.

1.4.5.6 The principal pedestrian access to tenancies adjoining the main street should be from the main street.

1.4.5.7 The gross retail floorspace of all tenancies directly adjoining the main street should generally be less than 1000m² GLA. Where the floorspace exceeds 1000m² the bulk of the tenancy should be sleeved behind other tenancies with floorspace less than 1000m².

1.4.5.8 Ground floor elevations to the main street should be glazed to a minimum of 70% of the building frontages, as a proportion of the total ground floor front elevation. Glazed areas are to be maintained free of obscure materials, coating or fixtures.

1.4.5.9 Buildings should not exceed three storeys, with the exception of those buildings identified as landmark sites on the centre plan.

1.4.5.10 A covered pick-up and drop-off point for disabled persons is to be provided at a suitable location to enable easy and convenient access to shops and services.

1.5 Planning Requirements for Precinct B – Business

1.5.1 Precinct B, focuses on the eastern end of Norton Promenade, forms a business gateway to the centre and allows for showrooms (including hardware and bulky goods) and other similar car-based business uses at the edge of the centre, in close proximity to Bussell Highway.

1.5.2 The following provisions apply to the area shown as "Precinct B" on the centre plan, in addition to the provisions of section 1.3.

1.5.3 Objectives

- a) Encourage general continuity of the built form along Norton Promenade and avoiding the use of blank walls.
- b) Accommodate the demand for showrooms (including hardware and bulky goods) and other low intensity non-retail and service uses at the periphery of the centre in proximity to Bussell Highway and Norton Promenade, and not in the retail core.
- c) Ensure car parking areas are appropriately located to minimise their

impact on the streetscape, while also acknowledging that access to, and visibility of, car parking areas is an important requirement for uses of this nature.

- d) Ensure appropriately managed and co-ordinated access to development from Norton Promenade, to avoid excessive disruption to traffic flow.
- e) Co-ordinate the scale and character of signage visible from Norton Promenade and Bussell Highway.
- f) Encourage a high quality streetscape, which includes significant tree planting, for Norton Promenade that is consistent with its role as a main entry road to the Dalyellup Estate.

1.5.4 Land Uses

1.5.4.1 The following land uses are “P” uses in Precinct B:

- a) Showroom
- b) Office
- c) Liquor Store
- d) Restaurant
- e) Take Away Food Outlet (excluding drive through fast foods)
- f) Auction Mart
- g) Club Premises
- h) Civic Building
- i) Civic Use
- j) Consulting Rooms
- k) Medical Centre
- l) Child Day Care Centre
- m) Health Studio
- n) Public Worship
- o) Car Park
- p) Veterinary Hospital

1.5.4.2 Other appropriate uses may be considered as “AA” uses at Council’s discretion where the use is consistent with the objectives of the precinct, and contributes to the local commercial character of the precinct.



1.5.5 Development Standards

1.5.5.1 No more than two rows of perpendicular car parking and one access aisle should be located between the building closest to Norton Promenade and Norton Promenade.

1.5.5.2 A covered pedestrian walkway should be provided to the façade of buildings fronting Norton Promenade and to public streets and public car parking areas. The awning should be designed at a pedestrian scale and provide an acceptable degree of shade and shelter.

1.5.5.3 The principal pedestrian entry to the buildings adjoining Norton Promenade should be on the building frontage to Norton Promenade.

1.5.5.4 The Council may require additional car parking provision over and above the normal standard for showroom uses in respect of a hardware store or similar use which may generate additional vehicle trips.

1.6. Planning Requirements for Precinct C – Civic

1.6.1 Precinct C provides a civic and community focus at the southern end of the main street with provision for a community purpose building and civic square.

1.6.2 The following provisions apply to the area shown as “Precinct C” on the centre plan, in addition to the provisions of section 1.3.

1.6.3 Objectives

- a) Provide a focal point and meeting place for the community through the development of a civic square, suitable open spaces and the provision of amenities.
- b) Accommodate a community purpose/civic building providing a visual end point to the main street and framing the civic square.
- c) Connect to the main street via clear pedestrian links, and a safe pedestrian environment.
- d) Include the provision of a landmark feature or similar to promote a sense of place.
- e) Provide the opportunity for the temporary closure of streets for use in conjunction with public events and celebrations.

1.6.4 Land Uses

1.6.4.1 The following land uses are “P” uses in Precinct C:

- a) Civic Building
- b) Civic Use
- c) Child Day Care Centre
- d) Market
- e) Public Recreation
- f) Car Park

1.6.4.2 Other appropriate uses may be considered as “AA’ uses at Council’s discretion where the use is consistent with the objectives of the precinct, and contributes to the civic and community character of the precinct.

1.6.5 Development Standards

1.6.5.1 On site car parking should not be located between buildings and the street. Large areas of car parking should be located behind buildings and landscaped where appropriate to provide additional shelter.

1.6.5.2 A covered pedestrian walkway should be provided to the front façade of the building to provide shade and shelter.

1.7 Planning Requirements for Precinct D – Residential and Mixed Use

1.7.1 Precinct D is a higher density residential and mixed use frame, providing a transition between the non-residential core of the centre and the surrounding residential areas.

1.7.2 The following provisions apply to the area shown as “Precinct D” on the centre plan in addition to the provisions of section 1.3.

1.7.3 Objectives

- a) Facilitate residential development and density commensurate with the size and scale of the centre, maximising the potential residential catchment of the centre.
- b) Encourage home based business and live – work housing, including premises which can evolve in time to small office accommodation.



- c) Encourage mixed use activities compatible and complementary to residential uses, typical of inner city residential areas.
- d) Provide a robust urban structure that allows for land use and density transition over time, recognising that the first generation of development particularly to the west and south of the precinct, is likely to be predominately residential.

1.7.4 Land Use

1.7.4.1 The following land uses are “P” uses in Precinct D:

- a) Single Dwelling
- b) Grouped Dwelling
- c) Multiple Dwelling
- d) Child Family Care Centre
- e) Guest House
- f) Home Occupation
- g) Office
- h) Consulting Rooms

1.7.4.2 Other appropriate uses may be considered as “AA” uses at Council’s discretion where the use is consistent with the objectives of the precinct, and does not compromise residential amenity or detract from the commercial viability of Precincts A and B.

1.7.5 Development Standards

1.7.5.1 The residential density code of Precinct D is R60.

1.7.5.2 Detailed area plans may be prepared for Precinct D to vary the acceptable development provisions of the Residential Design Codes consistent with the general and precinct-specific objectives.

1.7.5.3 Vehicle access and parking to residential dwellings is generally to be from the rear of the dwelling where laneways are provided.

1.7.5.4 Vehicle parking for commercial development is to be provided in the major off-street car parks or by on-street parking.

1.8 Planning Requirements for Precinct E: Residential

1.8.1 Precinct E provides a transition between the commercial and mixed use precincts and the residential areas outside of the immediate catchment to the District Centre. Precinct E is predominantly residential in nature.

1.8.2 The following provisions apply to the areas shown as “Precinct E” on the centre plan, in addition to the provisions of section 1.3.

1.8.3 Objectives

- a) Provide a higher density residential catchment close to the centre
- b) Create a high standard of residential amenity through the control of development and land use.

1.8.4 Land Uses

1.8.4.1 Land use permissibility is in accordance with the Residential zone of the scheme.

1.8.5 Development Standards

1.8.5.1 The residential density code of Precinct E is R60.

1.8.5.2 Residential development is to be undertaken in accordance with the Residential Design Codes and the provisions of Shire of Capel TPS7, unless otherwise specified in detailed area plans.



PART TWO: EXPLANATORY REPORT

2.1 Background

The Dalyellup District Centre will be the main activity centre serving the Dalyellup community and nearby areas. It will provide retail, office, community service, medical, entertainment and recreation opportunities for the district population, and will be the hub of community activities.

The Dalyellup area is already substantially developed and will ultimately comprise 6,000 dwellings with an estimated 15,000 residents.

2.2 Location

The site is located approximately 9 km south of the Bunbury city centre at the intersection of Norton Promenade, Parade Road North and Parade Road South, and comprises around 30 hectares (*Figure 3*).

The site consists of Lots 4550 and Pt Lot 9018 and is currently owned by Dalyellup Beach Pty Ltd the major developers of the Dalyellup residential community. DBLP is the 'owner' of the land for the purposes of clause 1.3.6.1.

The site and adjoining land is currently undeveloped.

There is constructed access to Norton Promenade and Parade Road North which are the major links into the Dalyellup estate.

2.3 Site Planning Considerations

The following site planning considerations, opportunities and constraints, have influenced the centre plan:-

- a) The site is relatively flat with a uniform elevation of 6 to 7 metres AHD and highly suitable for the development of a District Centre.
- b) Detailed environmental assessment of the site and surrounds has already been undertaken in previous structure planning for East Dalyellup. This has included detailed investigations of geology and soils, hydrology and surface drainage, wetlands, vegetation and flora, vegetation communities and contamination. There are no environmental constraints affecting future development of the site.



- c) The immediate boundaries of the District Centre are Bussell Highway and adjacent open space/overflow drainage corridor (east), vacant future residential land (south), a wetland and future open space reserve (west), and cottage and traditional residential lots (north). The Bethanie aged persons development is located north-west of the intersection of Norton Promenade/ Parade Road North.
- d) The Dalyellup East Local Structure Plan includes a comprehensive open space and recreation network. There are opportunities to incorporate attractive landscaped linkages into the design of the centre to connect to pathways into the wider open space network.
- e) Norton Promenade provides direct access to Bussell Highway via a traffic light controlled intersection. Parade Road (north of Norton Promenade) provides direct access to suburbs north of Dalyellup. There are proposals for the construction of Parade Road, south of Norton Promenade along the edge of the wetland to connect to Bussell Highway at Hastie Road and for an additional connection to Bussell Highway via Sleaford Park Drive by a traffic light controlled intersection.
- f) There are design constraints associated with the requirement to provide an overflow flood channel for the Five Mile Brook parallel to Bussell Highway which is proposed to be incorporated into an open space system forming an attractive and functional edge to the centre, and a safe and convenient footpath network.
- g) The wetland on the western boundary is of conservation significance and is to be retained providing an attractive outlook for nearby residential development.
- h) There is a need to address traffic noise impacts from Bussell Highway either by noise amelioration measures or by the location of non-sensitive uses adjacent to the Highway.
- i) Norton Promenade and Parade Road are major gateways into Dalyellup and it is important to ensure a high quality of builtform and landscaping along these roads.

2.4 Statutory Planning Framework

Greater Bunbury Region Scheme (GBRS)

The land is zoned Urban under the GBRS.

The GBRS provides that the approval of the WAPC is not required for development within the District Centre, except for retail development which is inconsistent with the centre plan.

Shire of Capel District Town Planning Scheme No. 7 (TPS7)

The site is zoned "Urban Development" under TPS7. The purpose of this zone is to designate land for future urban development and provide a framework for the preparation and approval of outline development plans prior to subdivision and development. The zoning is intended to be flexible in order to overcome the inherent problems associated with the detailed zoning of land prior to lot boundaries being established for subdivision and development.

Appendix 16 of TPS7 states:

"The subdividing landowners of Lots 803 and 9012 Norton Promenade shall make provision of (sic) the future development of a District Centre generally in accordance with an endorsed Local Structure Plan for this Precinct. In accordance with the Usher, Gelorup & Dalyellup District Structure Plan, the District Centre shall make provision for a maximum of 15,000 square metres of gross leasable retail floorspace area as well as provision for other uses such as, but not necessarily limited to, medical centre, service station and sites for other non-retail commercial uses such as a tavern, commercial recreation, service-commercial, showroom-warehouse, residential and offices".

The plan proposes up to 20,000m² retail floorspace consistent with the Interim Greater Bunbury Commercial Centres Strategy, the Bunbury-Wellington Regional Plan and Greater Bunbury Structure Plan. An amendment is to proceed concurrently with the centre plan to update TPS7.

Other relevant provisions of TPS7 are:-

- a) clause 5.10.3 which provides for the nomination of permitted uses on the centre plan;
- b) clause 5.10.4 which requires approval of the centre plan prior to the Council making recommendations to the WAPC on subdivision applications;



- c) clauses 5.10.6 to 5.10.10 which set out the requirements for the preparation, advertising and approval of outline development plans.

Appendix 16 also sets out specific development requirements for the District Centre including:-

- a) clause 1(c) which states the need to address noise generated by Bussell Highway;
- b) clause 1(h) which requires the ceding of a site of up to 1ha for municipal purposes;
- c) clause 1(l) which sets out responsibilities for the acquisition and construction of that portion of the proposed extension of Parade Road through land currently owned by the Riding for the Disabled Association of Western Australia (RDA).

With regard to c) above, the possibility of a land exchange involving Dalyellup Beach Pty Ltd, the RDA and Topshore Enterprises, owners of adjacent land, is being investigated with a view to alleviating the impact of the current alignment of Parade Road through the RDA land.

Dalyellup Beach Estate Local Structure Plan (2008)

The Dalyellup Beach Estate Local Structure Plan (DBELSP) was endorsed by the WAPC in May 1999 and applies to the western portion of Dalyellup, generally west of Dalyellup Boulevard (*Figure 4*). The Dalyellup Beach Estate is now substantially developed. A consolidation of the DBELSP was adopted in May 2008 incorporating various modifications which have been made since the plan was originally endorsed.

The location of the District Centre was initially on Harewoods Road but, following a review in 2003, was relocated to the present site for the following reasons:-

- a) A joint ministerial decision to remove the proposed extension of Ocean Drive to Centenary Road adversely affected the viability of the Harewoods Road site.
- b) An Enquiry by Design workshop concluded that the current proposed location would be more convenient and viable on the grounds that it:-
- is directly accessible to Bunbury via a possible future extension of Parade Road;

- provides a more prominent and more accessible position on Norton Promenade, the main entry road into Dalyellup; and
- is in a more central and convenient position to service Usher, Gelorup and Dalyellup.

Dalyellup East Local Structure Plan (2010)

The Dalyellup East Local Structure Plan (DELSP) applies to the eastern portion of the Dalyellup estate. The DELSP was endorsed by the Council and WAPC in November 2007. Since then a major modification (No. 12) has been approved by the Council and WAPC as well as a number of minor modifications. The DELSP was consolidated in 2008 incorporating all modifications to date (*Figure 5*).

Modification No. 12 confirmed the current boundaries of the District Centre, and major land uses including:-

- a) the realignment (split) of Parade Road south and north of Norton Promenade;
- b) the creation of a “main street” south of the Norton Promenade/Parade Road intersection;
- c) the concentration of commercial functions south of Norton Promenade;
- d) a public open space/drainage network along the Bussell Highway alignment.

A further modification to the DELSP is proposed to identify the District Centre and confirm that planning for the District Centre is subject to the centre plan.

2.5 Strategic Planning Framework

State Planning Policy No. 1: State Planning Framework (SPP1)

SPP1 sets out the key principles for land use planning and development which apply throughout the state. It brings together existing regional policies and plans into an ordered hierarchy to guide decision making on land use and development. Local governments are required to have “due regard” to the provisions of state planning policies in the preparation of planning schemes and consideration of planning matters.

The centre plan is consistent with the primary aim and key principles of SPP1 and the relevant planning instruments which are incorporated into SPP No.1 by reference.



State Planning Policy 4.2: Activity Centres for Perth and Peel (2010)

SPP 4.2 provides strategic guidance on the location of activity centres in metropolitan Perth and Peel. Whilst applicable to the metropolitan region, the objectives and general policy measures are also relevant to regional centres.

The provisions of the state policy which are relevant to the Dalyellup District Centre are:

- a) Centres should comprise a mix of uses that encourage activity outside normal business hours, provide local employment opportunities and high amenity public realm, and encourage multi-purpose trips, rather than being a single purpose shopping centre.
- b) The amount of land allocated to car parking should be minimised. The predominant builtform should allow the majority of buildings to be accessed via the public realm rather than separated by large areas of car parking.
- c) Bulky goods and showroom uses should be located on the periphery of centres accessible to the regional road network and public transport.
- d) Centre plans should optimise the potential for residential development in activity centres.

Interim Greater Bunbury Commercial Centre Strategy (2007)

This is an interim strategy setting out the WAPC's expectations for shopping development within the Greater Bunbury area until a more formal position is adopted. The strategy provides guidance on the distribution of retail floorspace to achieve the balanced provision of shopping facilities to serve the needs of Greater Bunbury.

The strategy identifies Dalyellup as a "major District Centre" with a maximum shopping floorspace of 20,000m² NLA.

The strategy further provides that local governments will normally determine developments that do not result in the projected floorspace figures being exceeded and where the proposed development is consistent with a local planning strategy or centre plan endorsed by the WAPC.

The strategy is based on the Bunbury-Wellington Region Plan and Greater Bunbury Structure Plan which designates the city centre of Bunbury as the regional centre with unlimited retail floorspace, complemented by two district centres at Dalyellup

and East Australind, each of approximately 20,000m² retail floorspace. This centre plan has been prepared with the potential for an ultimate 20,000m² retail floorspace consistent with the strategy.

Bunbury-Wellington Region Plan and Greater Bunbury Structure Plan (1995)

The Bunbury Wellington Region Plan includes the subject land within the Usher, Gelorup, Dalyellup Planning Unit BU6 and requires development to proceed in accordance with the approved Usher, Gelorup and Dalyellup District Structure Plan.

The region plan includes "District Centre" along with urban and mixed business as predominant land uses within Planning Unit BU6.

The Bunbury-Wellington Region Plan incorporates the Greater Bunbury Structure Plan which includes the District Centre site in a "Future Urban", (Category B) area.

Usher, Gelorup and Dalyellup District Structure Plan (1992)

The Usher, Gelorup and Dalyellup District Structure Plan provides a framework for urban expansion along the coastal corridor south of Bunbury. The plan proposed the District Centre abutting Harewoods Road south of the subject land. This centre was relocated to the present location on Norton Promenade in the DELSP (see above).

The Shire of Capel Land Use Strategy (April 1999)

The Shire of Capel Land Use Strategy applies to the entire municipal district and is incorporated into the TPS7 by the provisions of clause 6.1.

The Strategy includes the DELSP area within Planning Unit BU6 where the objective is to provide for urban expansion whilst conserving significant areas of natural and environmental value.

Liveable Neighbourhoods (2008)

Liveable Neighbourhoods (Element 7) specifically addresses activity centres. The following provisions are particularly relevant to the planning and design of the Dalyellup District Centre:-

- a) Traditional main street fronting layouts should predominate focusing on the public realm as opposed to enclosed shopping malls although a limited amount of floorspace can be provided in an enclosed centre.



- b) Centres should be planned as mixed-use activity centres incorporating local offices, business, community and other complementary uses, and not be just shopping centres.
- c) Car parking may be reduced where there is reciprocal use and on-street parking.
- d) Off-street parking should be located behind buildings.
- e) Centres should have an appropriate range of higher density housing in and around the centre core with densities of 30-40 dwellings per site hectare within 400m of centres.
- f) Centres should be designed to encourage walking, cycling and access by public transport.
- g) To achieve good streetscapes, appropriate building types and designs are needed, with a particular focus on the ground floor, to support better urban amenity.

2.6 Community Consultation

A community workshop was held in August 2007 to obtain feedback from the community on the future planning and design of the centre.

The following feedback was received at the community workshop:

- the District Centre should be a “town centre” not just a “shopping centre”;
- it should be a community hub for a mix of uses including retail, commercial/office, residential and civic functions;
- the core of the centre should be an attractive and active main street with street trees;
- the centre should maximise opportunities for a wide trading and activity envelope (day and night);
- the centre should be pedestrian-friendly and accessible to cyclists;
- it should be a place that is a destination, to meet and enjoy life;
- building heights should not exceed 3 storeys;

- on-street parking should be provided and the main off-street car parks should be screened by buildings from the streets and public realm; and
- provision should be made for a town square and landscaped open space.

The initial community feedback was taken into account in the preparation of the centre plan.

There was further opportunity for public comment during the statutory advertising of the centre plan between January-March 2011. The community consultation included:

- printing and publication of 5,000 copies of a brochure outlining the centre plan proposals and advising of the opportunity to comment;
- a letterbox drop of 3,500 copies of the brochure to residents of Dalyellup and Gelorup;
- a community information day held on Saturday, 5 February 2011 at the Dalyellup Beach Community Centre.

From the comments received at the community information day, there was general support for the concept plan and the proposed development. The most frequent comment was the need to proceed with the development as soon as possible. There were mixed views on the tavern with some supporting a "family friendly" tavern as a local amenity and meeting place, whereas others were concerned at possible anti-social behaviour. Specific priorities mentioned included a supermarket, post office, hardware store and skate park. There was also particular mention of a need for development to be "seniors friendly" and to cater for the disabled with a covered disabled drop off point which has been accommodated in the plan.

2.7 Retail Potential Assessment

Shrapnel Urban Planning undertook a retail assessment for Dalyellup District Centre in May 2007 which recommended 17,450m² retail floorspace in the main street core with around 15,000m² shopping and the balance commercial non-retail, consistent with the current provisions of TPS7. The proposed mix included two supermarkets, a discount department store and specialty shops.

The Shrapnel assessment has been reviewed and updated by Urbis Property Economics in November 2010 and again in March 2011 (Appendix 1). Current estimates indicate that ultimately there is a need for up to 20,000m² retail floorspace which



would be consistent with the Interim Greater Bunbury Commercial Centres Strategy. This will require an amendment to TPS7 which is to proceed concurrently with the centre plan.

2.8 Vision and Objectives

The vision for Dalyellup District Centre is:-

Dalyellup District Centre will be an attractive, diverse, safe and sustainable activity centre with a unique urban village character and will provide a focus for shopping, business, community and recreation activities for the use and enjoyment of the local community and wider district.

This vision translates into the key general centre objectives and more specific precinct objectives which are set out in Part 1.

2.9 The Concept Plan

The concept plan for the Dalyellup District Centre (*Figure 2*) shows the proposed pattern of land uses, built form and public spaces. The concept plan reflects the feedback received from the community workshop, current planning policy, and contemporary urban design practice.

Dalyellup District Centre will be developed around a traditional main shopping street with pedestrian-friendly streets lined by attractive buildings and with convenient, comfortable and attractive public spaces. It will contain shops, offices, showrooms, cafés, restaurants and community activities located near each other to enhance the vitality and attractiveness of the centre. A variety of housing types, including housing mixed with office uses, will offer a range of housing choice. Housing will be designed to maximise safe and convenient access to local services and will contribute further to the vitality and strong sense of community wellbeing.

Whilst the centre is intended to be developed with a wide range and mix of uses, some concentration of land use and built form into precincts with common characteristics is proposed to capitalise on synergies between complementary land uses and to create variety and character in the urban form. The centre comprises five precincts as follows:-

- a) main street retail core;
- b) business precinct;

- c) civic and community precinct;
- d) residential and mixed use precinct; and
- e) residential precinct.

An indicative design of the Main Street is shown in Figure 2A.

Photographs of the type of development envisaged for the various precincts (*Figures 6 to 10*) and an artist's impression of the main street (*Figure 11*) are attached.

The main street precinct is the main focus of shopping and associated activities. A central north-south "main street" intersects with an east-west pedestrian link providing an active public realm with a pedestrian square at the shopping hub of the centre. The main street and east-west link will be activated with shopfronts and other street-based uses (ie. uses that open out into the street such as alfresco dining), providing vitality and legibility. The activated street frontages will also heighten the sense of community and provide the opportunity for businesses to operate outside conventional trading hours. The retail core will include two supermarkets and a discount department store. Specialist shops will sleeve the frontages of the supermarkets and discount department store to provide streetfront activity.

The business precinct is located to the east of the main street core towards Bussell Highway and with frontages along Norton Promenade. The business precinct is an important gateway to the centre and will include a range of showrooms (including hardware and bulky goods), offices, consulting rooms and local service type uses aimed primarily at serving the local market. The concept plan shows provision for a hardware/bulky goods store east of the neighbourhood connector.

The main street will culminate at its southern end in a civic precinct. This will provide the focus for community activity and will be the public heart of the centre. The civic precinct has a civic square framed by a civic building providing a focal point and sense of place. The civic building will also provide a visual anchor to the main street.

The residential and mixed use precinct provides a transition between the main street and business precincts and the surrounding residential areas. This precinct will consist of a range of medium density housing, including two storey terraced townhouses, with opportunities for home-based business activities. There will also be opportunities for mixed-use activities compatible and complementary to residential uses particularly on the Norton Promenade frontage.



The residential precinct is intended to accommodate a variety of housing types with an emphasis on transitioning towards traditional single houses. Residents will have excellent access to the shopping, leisure and community facilities within the centre and will also support these local services. A high level of amenity and security will be designed into the residential buildings, public open space and other public spaces. The residential precinct will provide a transition between the District Centre and adjacent residential areas to the north, south and west of the centre.

Buildings throughout the centre will not exceed three storeys.

Every opportunity will be taken to provide on-street parking, both on the main street and on other streets in the District Centre. The main off-street parking areas are located where they are screened from the surrounding street network. Provision is made for reciprocal parking to maximise the use of available spaces.

Pedestrian circulation will be convenient and safe, and there will be good access from neighbouring areas for pedestrians, cyclists, public transport and motor vehicles.

A key element of the concept plan is a significant east-west pedestrian corridor linking the main street and town square to the wetland and public open space west of Parade Road South.

The design of buildings, streetscapes, and the public realm will be of a high standard to ensure the primary design themes of creating an attractive and vibrant centre which fits well into its setting in the new and emerging Dalyellup community, are achieved.

2.10 Precinct Descriptions

This section describes the objectives, land use, built form and structural elements of each precinct and the statutory development standards contained in Part 1.

Precinct A – Retail Centre Core

Precinct A is the retail centre focussed on a north-south main street connecting Norton Promenade at the northern end to the civic and community precinct to the south.

The objectives of the retail core are to:

- Create a pedestrian-friendly main street retail core with high amenity and a strong sense of place.
- Encourage a mix of compatible shopping and related uses including uses that generate activity outside core business hours.
- Encourage richness in the streetscape, including articulation of buildings, windows and openings to create visual interest at street level.
- Facilitate strong and direct pedestrian connectivity to, along and across the main street.
- Encourage a continuous paved and covered pedestrian walkway along the edge (verge) of the main street.
- Provide intimate community meeting places in public areas with seating and other amenities.
- Allow on street parking where possible. Encourage large areas of off street parking to be located behind buildings adjoining the main street.
- Enable temporary activities in the street, such as alfresco dining and external displays, to create interest.

Land Use:

Land uses within the centre core will be predominately high activity generating retail, with complementary uses providing entertainment, leisure and commercial services.

Permitted uses, suited to the retail character of the precinct, are: shop (including supermarket and department store), restaurant, office, take away food outlet (excluding drive through fast foods), liquor store, tavern, betting agency, art gallery, art and craft studio and sales, public amusement, amusement parlour, consulting rooms, medical centre, health studio, car park and multiple dwelling. Other uses that promote the shopping and main street role of the precinct can be considered by Council as "AA" uses.

Larger floorspace tenancies that exceed 1000m² in retail floorspace, such as supermarkets and discount department stores, are to be screened behind specialty stores of smaller floorspace directly fronting the main street. This prevents large stretches of the main street being dominated by a single land use that does not engage with the main street.



Residential uses are not permitted on the ground floor fronting the main street, to avoid inactive building frontages. Multiple dwellings are permitted above ground floor retail and office uses, increasing the potential for activity outside core hours and maximising residential catchment to the centre. Single and grouped dwellings are not permitted within the centre core, as they are not consistent with predominantly shopping, and main street character of the precinct.

Built Form:

There are a number of core built form elements required through the statutory provisions to ensure the delivery of a quality main street that integrates with and engages the public realm.

- Landmark sites are identified at the northern end of the main street, at the intersection of Norton Promenade. The development of landmark buildings on these sites will aid legibility and sense of place. Acceptable design features include: architectural features, glazing, public art, landscape features, materials, colour, height, scale, major openings, balconies and other applied structures.
- A nil setback applies to the buildings fronting the main street, ensuring the buildings relate to the public realm. This setback may be varied to allow features that create interest and amenity, including forecourts, alfresco dining and architectural features.
- Buildings are to have a generally continuous frontage to the main street, facilitating the creation of intimate urban spaces. Breaks in the building frontage are permitted for vehicular access and servicing, in accordance with the preferred crossover locations, or to provide articulation and interest in the streetscape.
- Principal pedestrian access to retail tenancies adjoining the main street will be from the main street, avoiding dead facades and tenancies only opening from an internal mall or car parking area.
- The ground floor elevation to the main street is required to be glazed to a minimum of 70% of the building frontage. This ensures the building design addresses and fronts the main street, avoiding inactive frontages. The glazed frontages should contain displays for visual interest and not be obscured by materials, coatings or fixtures.
- Service areas are to be generally screened from public view, preferably at the rear of buildings so as not to impact on the amenity and streetscape of the public realm.

Structural Elements and Networks:

The centre core is focussed on a north-south main street spine, connecting from a signalised intersection at Norton Promenade at the northern end to the civic precinct and tree lined pedestrian connection at the southern end.

There are two typical cross sections for the main street:

- The central portion of the main street is contained within an 19.6m reserve comprised of a central 6.4m carriageway, on street parking and 4.1m wide verges. The verge width provides sufficient space for alfresco dining and other temporary activities. This tight urban main street cross section, located in the vicinity of the town square, contributes to the slowing of traffic, intimacy of the public realm and the connection between the built form and adjoining land uses with the main street. Pedestrian crossing points will be created in this narrowed zone.
- At the northern and southern ends the main street widens to 29.4m to accommodate central median parking. The central parking assists in slowing traffic and dispersing car parking, rather than relying solely on large single use parking areas.

The main street design is shown conceptually in Figure 2A and described in more detail in Appendix 2: Traffic and Transport Report (5.3.2).

A town square is located central to the main street, providing the opportunity for a community meeting place and local amenity for restaurants and cafes to open onto. An east-west pedestrian link connects the town square with the parking areas to the rear, with the potential for tenancies to open onto this walkway. To the west, the pedestrian link connects with the wetland and public open space.

The town square will include opportunities for casual seating, shade/shelter, public art and planting. The town square could be used for community activities, such as art displays, street entertainment and fundraising activities. The landscaping treatment of this space is of primary importance to provide local amenity, and an attractive focal point to the main street.

Vehicle crossovers to the main street are limited to preferred locations at the northern and southern ends, providing access to the car parking at the rear of the main street buildings. Crossovers are limited to reduce the breaks in the main street built form character. Additional crossovers or refinements to the preferred crossover locations may be considered as part of the development application process. Additional crossover locations are to be in keeping with the precinct objectives.



Car parking:

On street parking both within the verge and the median, will be provided on the main street and surrounding roads. On street parking slows traffic and disperses parking to minimise the size of large single use car parking areas.

Permanent off street car parking areas are not permitted between the building and the main street, and are to be located behind buildings and screened front the main street.

Precinct B – Business

Precinct B is the business gateway to the centre, providing an opportunity for car based showroom and bulky goods uses to be located at the edge of the centre, in close proximity to Bussell Highway.

The objectives of the business precinct are to:

- Encourage general continuity of built form along Norton Promenade.
- Accommodate the demand for showrooms and other low intensity non-retail and service uses on the periphery of the centre in proximity to Bussell Highway and Norton Promenade.
- Ensure car parking areas are appropriately located to minimise their impact on the streetscape, while also acknowledging that access to , and visibility of, car parking areas is an important factor for uses of this nature.
- Ensure appropriately managed and co-ordinated access to development from Norton Promenade, to avoid excessive disruption to traffic flow.
- Co-ordinate the scale and character of signage visible from Norton Promenade.

Land Use:

Land uses within the business precinct will be predominately showroom and bulky goods uses such as auto service and accessories, kitchen and bathroom supplies, hardware, garden supplies or pool services. These uses are typically not suited to the main street environment, because the built form and reliance on car parking at the entry is inconsistent with the main street, pedestrian character of the main street.

Permitted land uses are: showroom, office, liquor store, restaurant, take away food outlet (excluding drive through fast food), auction mart, club premises, civic building,

civic use, consulting rooms, medical centre, child day care centre, health studio, public worship, car park and veterinary hospital. Other land uses consistent with the local commercial and business character of the precinct may be considered by Council as "AA" uses.

The advertised concept plan showed showroom units on that part of the business precinct east of the neighbourhood connector road. This has been modified to show the option of a hardware/bulky goods store which is within the definition of a "showroom" under TPS7. The alignment of the neighbourhood connector and the site configuration were adjusted to accommodate the hardware/bulky goods use and the local access street removed and incorporated into the circulation system of the site. A review of the original traffic assessment (Appendix 2) was undertaken and the results are documented in the Traffic and Transport Addendum Report (Appendix 2A). This concluded that the traffic impacts associated with a hardware/bulky goods store can be satisfactorily managed.

Built Form:

As the gateway to the District Centre it is important that buildings extend as far east as possible to take advantage of exposure to Bussell Highway and visually announce the centre by addressing and relating to the public realm.

A covered pedestrian walkway should be provided on the front elevation of buildings adjoining the street, including Norton Promenade other than in front of the hardware/bulky goods store. The awning is to be designed at a pedestrian scale, providing shade and shelter.

The principal pedestrian entry to the buildings on Norton Promenade is to be from the frontage adjoining Norton Promenade, avoiding dead facades to the street.

Service areas are to be generally screened from public view, preferably at the rear of buildings so as not to impact on the amenity and streetscape.

Car Parking:

To ensure buildings address the public realm and avoid large expanses of car parking dominating the entry to the centre, no more than two rows of perpendicular car parking and one access aisle is to be located between the building frontage and Norton Promenade. The remaining car parking is to be located to the rear of the buildings.



The Traffic and Transport Addendum Report (Appendix 2A) assumes a parking requirement at 80% of the standard rate for shopping (1:20m²) for a hardware/bulky goods store. This equates to 1:25sqm and is greater than the parking standards for showrooms in TPS7 (1:40m²) because of the more intensive sale of goods relative to regular showroom activity.

Precinct C – Civic

Precinct C is located at the southern end of the main street, providing the opportunity for a community purpose building and civic square close to the parkland along Bussell Highway. Precinct C is 1 hectare and is to be provided free of cost to the Shire of Capel for community use, consistent with the requirements of TPS7.

The objectives of the civic precinct are to:

- Provide a focal point and meeting place for the community through the development of suitable open space and the provision of amenities.
- Accommodate a civic square and a community purpose/civic building, providing a visual end point to the main street and framing the civic square.
- Connect to the main street via clear pedestrian links, and a safe pedestrian environment.
- Include the provision of a landmark feature or similar to promote a sense of place.
- Provide the opportunity for the temporary closure of streets for uses in conjunction with public events and celebrations.

Land Use:

Land uses within the civic precinct will have a community focus, with the uses determined by the Shire of Capel. The civic square and nearby open space provide a venue for local markets.

Permitted uses are: civic building/civic use, child day care centre, market, public recreation and car park. Other uses consistent with the community purpose of the site can be considered by Council as "AA" uses.

Built Form:

The civic precinct is the southern anchor for the main street. The built form and landscaping on the site is to recognise and relate to the main street and the east west landscaped pedestrian connection, in particular:

- Large areas of off street parking should not be located between the building and the street, and should be located behind buildings and landscaped. This is to avoid large expanses of car parking separating the civic precinct from the main street.
- The north-western corner of the civic precinct is recognised as a landmark site, facilitating the delivery of built form and /or a landscape feature that builds legibility and book ends the main street. This could include public art in the civic square or an architectural feature on the civic building.
- A covered pedestrian walkway is to be provided to the front facade of the civic building for pedestrian shade and shelter.

Car Parking:

Car parking requirements for civic uses are in accordance with the provisions of TPS7.

Precinct D – Residential and Mixed Use

Precinct D is the residential and mixed use frame to the centre, providing a transition between commercial and residential uses. Precinct D is located north of Norton Promenade and also wrapping around the edge of the retail core to the north, south and west.

The objectives of Precinct D are to:

- Facilitate residential development and density commensurate with the size and scale of the centre, and maximising the potential residential catchment of the centre.
- Encourage home based business and live-work housing, where the premises can evolve in time to small office accommodation.
- Encourage mixed use activities compatible and complementary to residential uses, typical of inner city residential areas.



- Provide a robust urban structure that allows for land use and density transition over time, recognising that the first generation of development is likely to be predominately residential.

Land Use:

The residential and mixed use precinct will provide a transition between the retail and residential uses, generating local employment and provision of local services.

Permitted uses within Precinct D are single, grouped and multiple dwellings, consulting rooms, guest house, home occupation, child family care centre and office. Other uses consistent with the residential and mixed use character of the precinct can be considered by Council as "AA" uses.

Non-residential uses such as consulting rooms and offices are likely to be attractive north of Norton Promenade, given the exposure to passing traffic and direct relationship to the business precinct.

Residential and home occupation or home business uses are likely to be more common where Precinct D directly adjoins the retail core. There is the potential for these lots to have a second home occupation entrance to the rear, accessed from the laneway and car parking area.

Built Form:

Precinct D is coded Residential R60 and lots will be typically between 240-360m², with access via a rear laneway.

Built form within Precinct D will generally be of a more mixed use character north of Norton Promenade and a more residential character elsewhere in the precinct. Residential densities will be in accordance with the R60 requirements of the Residential Design Codes. Detailed area plans may be prepared for Precinct D to vary the acceptable development provisions of the Codes for all or part of the precinct.

Car Parking:

Car parking for residential uses will be off street, accessed via the rear laneway and provided in accordance with the Residential Design Codes.

Car parking for non-residential uses can be located on street, adjoining the property, and provided in accordance with the TPS7.

Precinct E – Residential

Precinct E is located on the periphery of the centre plan area and directly integrates with the surrounding residential development and existing residential character.

The objectives of Precinct E are to:

- Provide a residential catchment close to the centre.
- Create a high standard of residential amenity through the control of development and land use.

Land Use:

Precinct E will be predominately residential with the potential for some home occupation uses. Permitted land uses are in accordance with the Residential Zone of the TPS7.

Built Form:

Precinct E is coded Residential R60. Residential development will be medium density, predominantly single residential, traditional and cottage lots. The R60 coding will also provide the opportunity for grouped housing, townhouses and low rise apartments and maisonettes.

2.11 Traffic and Access

Sinclair Knight Merz Transport Consultants has undertaken an assessment of the transport and access arrangements for the proposed District Centre which is presented in Appendix 2. An addendum report which provides for the introduction of a hardware/bulky goods store of up to 13,000m² on the portion of the business precinct east of the neighbourhood connector in place of showroom units and incorporation of the eastern local access street into the site, was later undertaken and is attached at Appendix 2A. The assessments followed the WAPC guidelines for transport assessments and were prepared in consultation with the Department of Planning..

The District Centre is projected to generate up to 25,000 vehicular trips per day. This includes around 8 vehicular trips per day per dwelling.

A hierarchy of roads and streets is proposed comprising the main street, which will be specifically designed as a pedestrian-friendly street, and other streets which will be designed according to their function based on Liveable Neighbourhoods.



The main street has been designed as a traffic-calmed environment flanked with high quality pedestrian spaces. Kerbside and median parking is provided on the main street in appropriate locations. This street is estimated to carry between 1,200 and 6,600 vehicles per day with the highest volumes at the Norton Promenade end.

Norton Promenade and Parade Road are classified as Integrator Arterial B Roads which will carry the majority of through and district traffic. The ring road around the retail core is classified as a neighbourhood connector and will be a key route providing access to the centre and its associated car parks. Various access streets provide direct access to properties with relatively low traffic speeds and volumes, and are designed to facilitate pedestrian and bike movements. The system of laneways will provide the main access to car parks and rear access to properties.

The main access to the District Centre will be via Norton Promenade, a two lane divided road, which could carry up to 12,900 vehicles per day at its busiest section in the vicinity of Bussell Highway. Access to Norton Promenade will be restricted.

Access into the main shopping carparks and service bays will also be from Parade Road South and the ring road through Precinct D (Residential Mixed Use). These will be important traffic and pedestrian intersections and will need to be designed to provide safe vehicular and pedestrian movement, including service vehicles.

The centre will be served by two planned bus routes with a timed point near the intersection of Norton Promenade and Main Street. The Public Transport Authority has advised that, one of these routes, the existing 401 route to Dalyellup, is proposed to be redirected along Parade Road and Norton Promenade in late 2010.

A range of route choices will be provided for pedestrians and cyclists to, from and through the centre. Pedestrian crossings are proposed along key desire lines.

2.12 Car Parking

The car parking arrangements for the District Centre are described in detail in Appendix 2.

TPS7 currently requires 1 bay per 20m² of GLA for shop and office uses, one bay for every 40m² GLA for showrooms, with car parking for other uses such as restaurants, cafe and consulting room calculated based on detailed criteria such as the number of seats or practitioners.

The best estimate of the parking requirement based on the proposed concept plan is approximately 1,600 bays for retail, business, mixed use and other non-residential uses, and 1,865 bays with inclusion of a major hardware/homemaker store. The concept plan shows the potential to accommodate 1,665 off street and over 200 on street bays sufficient to accommodate the expected demand. The ultimate actual parking yield as opposed to the estimate of parking based on the concept plan may be less as a result of detailed design to comply with Scheme standards, to make proper provision for adequate landscaping, provide pedestrian paths and thoroughfares and allowance for the movement of supply and service vehicles. Consequently as stages of development proceed the demand for parking from the use of various classes of floorspace will have to be carefully assessed and full development of the Retail Core and Business Precincts will be contingent on confirmation that adequate parking will be provided.

In designing the District Centre, on-street kerbside parking should be provided wherever possible. Both kerbside and median parking is proposed on the main street. Kerbside parking is also proposed in controlled access parking bays along Norton Promenade and Parade Road. The major off-street car parks are in convenient locations, accessible from the main street and ring-road with direct pedestrian access to the main street. The off-street car parks are generally screened by buildings and not visible from the main street.

The centre plan allows for reciprocal use of parking bays between different uses at different times consistent with current policy. TPS7 does not specify car parking concessions where there is reciprocal use and sharing of trips. It is anticipated, subject to more detailed planning, that reciprocal parking could reduce the non-residential parking requirement by around 20%. Reciprocal parking arrangements are proposed where it can be demonstrated that the demand for parking facilities serving separate uses or a mixed development will not coincide.

The Part 1 Statutory Provisions allow for concessions in respect of car parking requirements at the development applicant stage subject to satisfying the following:

- the development generally has an active presentation to the public realm;
- car parking is located in accordance with the precinct-specific development standards;
- nearby on-street or reciprocal use of off-street parking is available.
- an adopted parking management plan for Precincts A to C.



2.13 Servicing

The servicing strategy for the District Centre is set out in detail in Appendix 3.

Drainage and water management

The strategy for drainage management and servicing for the centre is addressed in the DELSP.

A water management plan has been prepared for Dalyellup East (2006) which sets the framework for drainage design for the District Centre. The drainage strategy comprises attenuation of flows on site by retention basins located in public open space to limit the flow from the northern boundary of East Dalyellup to the pre-development flow. Surface drainage will be by a conventional system of road gullies and underground pipes draining to the retention/infiltration basins where recharging into the groundwater will be possible. In areas of high post-development groundwater, subsoil drainage will be installed to provide adequate clearance between the ground water and building levels. Subsoil drains will be constructed at pre-development average and annual maximum groundwater levels.

The district drainage strategy involves a 20 metre wide overland flow channel alongside Bussell Highway to cater for any overflow from the Five Mile Brook. Culverts will be installed under Norton Promenade to convey flows under the road at this location. The report "Five Mile Brook-East Dalyellup Flood Relief Flow Plan: Implementation Plan" (2007) describes the proposals for the construction of the overland flow path.

Sewage

The Water Corporation has made provision for sewage from the District Centre in its planning for Dalyellup. The sewer is to gravitate into a proposed Type 180 pump station located in the public open space just south of the intersection of Norton Promenade and Bussell Highway. From here it is pumped to the wastewater treatment plant north of Dalyellup.

A main sewer is required to be constructed within the ring road surrounding the District Centre. The sewer flows from the District Centre will grade to this main sewer.

There are no sensitive land uses within the Type 180 pump station buffer zone.

Water Supply

The Water Corporation has made provision for water supply to the District Centre in its planning for servicing the area. The District Centre will be supplied from the existing 200mm diameter water main in Norton Promenade with 150mm and 200mm feeder lines.

Underground Power

Underground electricity supply for the District Centre will be fed from the existing main HV feeder along Norton Promenade. Western Power are currently reviewing if this supply will be sufficient to service the entire District Centre. If this supply is not sufficient, a new supply will be required to be laid to Dalyellup. A main HV cable will be required along Parade Road, south of Norton Promenade. Sites for switching and transformer padmounts will be set aside on subdivision and development stage.

Gas Supply

Gas supply for the District Centre will be fed from the existing 150mm diameter gas main along Norton Promenade.

Telephone Communications

Telephone communications will be extended from the existing infrastructure in Norton Promenade. Telstra are currently installing "fibre to the home" for the Stage 18 development north of Norton Promenade. It is proposed to extend this system to the District Centre.

Waste Storage and Collection

Consideration will be given to the method of collection of waste both with regard to putrescible and recyclable waste streams in the design of the road network, access ways, parking areas and buildings along with provision for access for loading bays and service yards.

2.14 Sustainable Development

The centre plan is consistent with current policy including the WAPC's Liveable Neighbourhoods and with the underlying principles set out in the draft Activity Centres for Greater Bunbury Policy (June 2011). It provides guidance on sustainability initiatives which will be implemented in more detailed planning and design.



The DELSP provides a sound basis for the achievement of sustainability outcomes in the location and context for the centre in the following ways:

- a) Providing a multi functional community focussed town centre that provides far more than just a shopping centre through the inclusion of business, employment, community uses and opportunities for social interaction in a highly accessible location.
- b) Providing high density living within the walkable catchment of the centre, reducing the reliance on private car travel.
- c) Establishing a highly permeable footpath and cycleway network to and from the centre, together with convenient public transport, further reducing the dependence on private cars.
- d) Establishing a public open space network which links surrounding residential areas with the centre and provides an attractive landscape based on water efficient design.
- e) Defining activity centre boundaries which will prevent the unplanned growth and expansion of the centre.

The centre plan addresses sustainability initiatives directly through:

- a) a mix of land uses;
- b) the concentration of shopping, civic and community uses on the main street in a compact urban form;
- c) the location of low intensity business uses outside the core;
- d) R60 residential density;
- e) provision of local employment;
- f) prioritisation of public transport;
- g) easy access for pedestrians and cyclists including end of trip facilities;
- h) common use of car parking;
- i) a quality public realm and a sense of place and ownership;
- j) energy efficient building orientation and design;
- k) provision for water saving and reuse of water in landscaping and buildings.

The centre plan contains a clear set of objectives showing how the development of the centre will address energy and water conservation at a more detailed planning level, including energy efficient building design and layout, and water conservation through water sensitive urban design.

The Building Code of Australia also prescribes standards for buildings to improve energy efficiency.

The centre plan requires the preparation of a sustainability outcomes and implementation plan together with audit reports on completion of each stage of development. This will ensure the translation of sustainability principles into action during detailed building design and site development. It will include consideration of measures such as energy efficient ventilation, low energy lighting, insulation, low energy heating, water efficient building fixtures, low water use landscaping, strategic shading solutions, water sensitive urban design and efficient waste management.

2.15 Implementation

The Centre Plan provides the framework to guide development of the district centre. It will also be important to establish mechanisms to ensure the successful implementation of the plan.

Staging

The district centre land is presently owned by Dalyellup Beach Pty Ltd, a joint venture between Satterley Property Group, Department of Housing and Works (DHW) and a syndicate of private investors. It is currently proposed that the shopping and business components of the district centre will be sold to a specialised commercial developer(s) to undertake the development. This will enable DHW to concentrate on its core business of housing provision. Subdivision approval will be required to enable the transfer to take place and to separate the ownership of the shopping and business components from the civic and residential components.

It is anticipated that development will be staged with the fundamental driver being the intentions of the new owner(s) and major tenants intending to locate in the centre. It is not therefore, possible to be precise about the staging at this time.

The centre plan, however, requires the preparation of a staging strategy prior to subdivision and development. The staging strategy will need to be monitored and updated on a regular basis.

The staging plan will show:

- a) the likely timing and staging of subdivision and development;



- b) the obligations and responsibilities of the different owners in respect of such matters as the construction of subdivision work and services, the preparation of more detailed plans and strategies which are required under the centre plan, transfer of the community purpose site to Council, and construction of Parade Road through the RDA land.

The centre plan requires that the Stage 1 development is to include a supermarket and construction of the main street.

The land comprising the civic precinct is to be transferred to the Council for civic and community purposes and the timing of development will depend on the proposals and program of the Council.

More Detailed Plans and Strategies

The centre plan sets out the broad framework for the future development of the District Centre. More detailed plans and strategies will be required to ensure the successful implementation of the plan. These include:

- a) a place management plan based on a co-ordinated proactive partnership between the developers and the Council to ensure the Dalyellup district centre is a desirable and attractive place, implemented through initiatives such as marketing and branding, maintenance and management of public and private spaces, promoting public events and presentations, safety and security programs, management and administration of public markets, and the review and monitoring of parking policy.
- b) a landscape strategy for the public realm and system of public open spaces which is complementary to the design theme for the centre, links precincts in a unified way, creates attractive open spaces of passive and active recreation, adds to the richness of the urban fabric, provides street trees to give shade and soften the built form, and reinforces crime prevention through the design of buildings and public spaces.
- c) A signage strategy to ensure advertising signs are co-ordinated and coherent, complement and reinforce the character of the precincts and the centre as a whole, and avoid clutter.
- d) A Sustainability Outcomes and Implementation Plan which will set out the proposed measures to achieve sustainable outcomes in more detailed building design, construction and management.

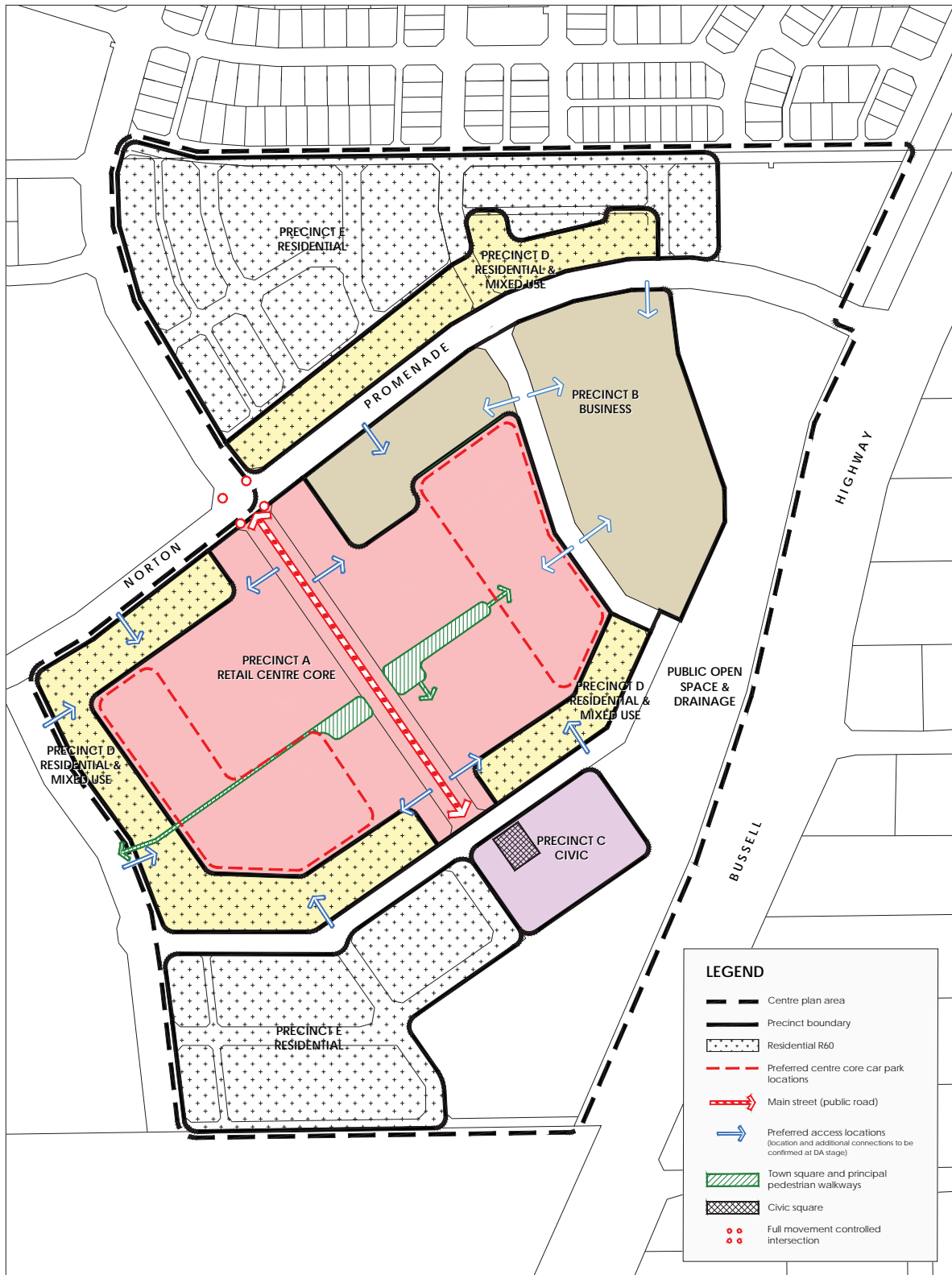
- e) A Staging Strategy to provide for the effective co-ordination and staged implementation of the centre plan.

Other more detailed plans and strategies may be required for the effective development of the centre including urban design and built form guidelines, ongoing parking management, infrastructure and development maintenance, waste management, and the temporary treatment of unused or undeveloped land. Some of these plans or strategies may be combined or form part of the aforementioned plans.



FIGURES

Dalyellup District Centre Outline Development Plan



DALYELLUP DISTRICT CENTRE
Draft Centre Plan
Shire of Capel



plan no: 2118-13G-01
scale: not to scale
date: 11.06.2012

T: (4618) 9382 1233 F: (4618) 9382 1127
E: admin@cleplan.com.au
www.cleplan.com.au
L2-38 ROWLAND STREET SUBURBO WA 6008
PO BOX 798 SUDBORO WA 6054

This plan is current at the revised date & subject to approval, survey & engineering detail. This plan remains the property of CLE ©

Figure 1 - Centre plan

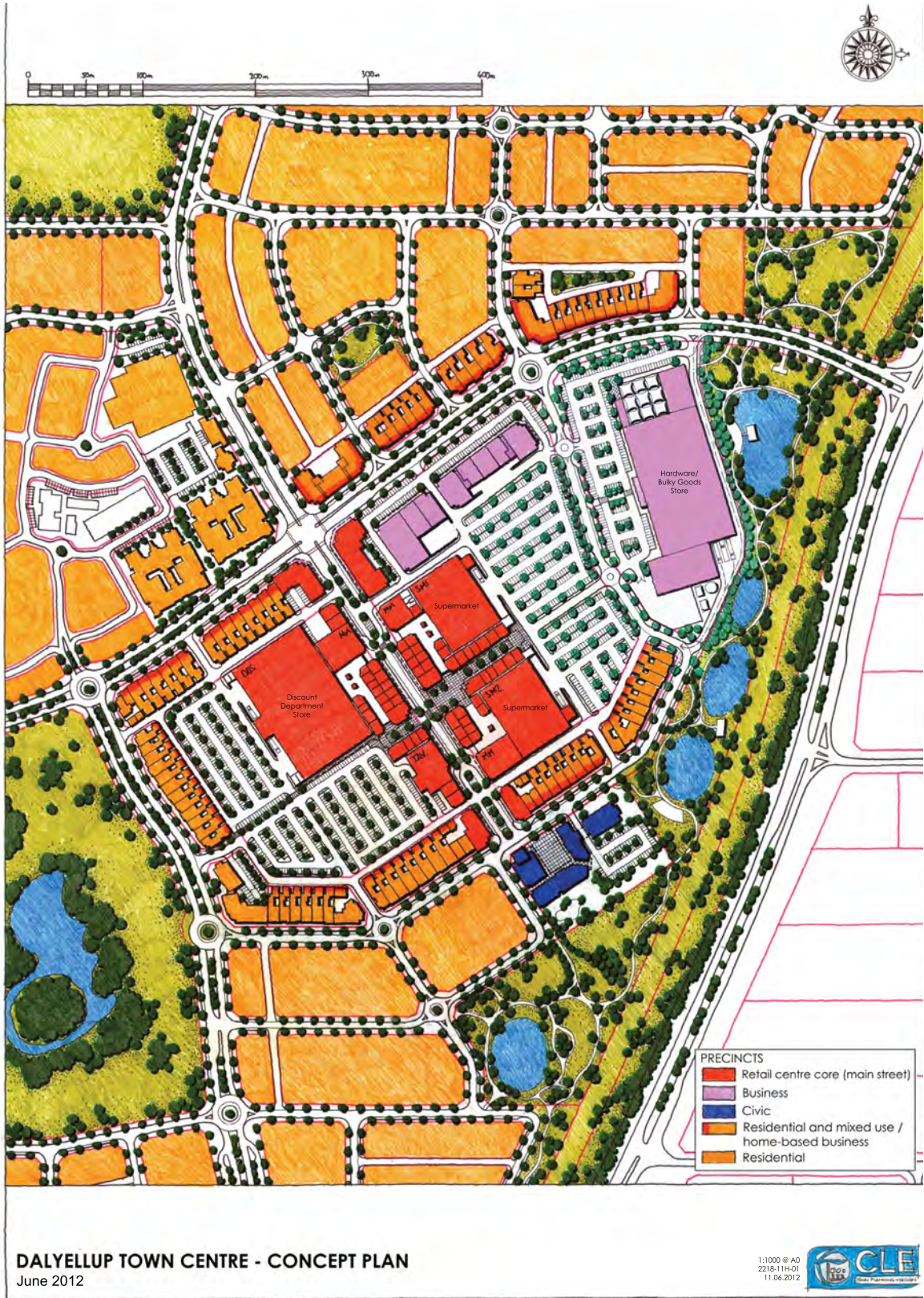


Figure 2 - Concept Plan

Norton Promenade

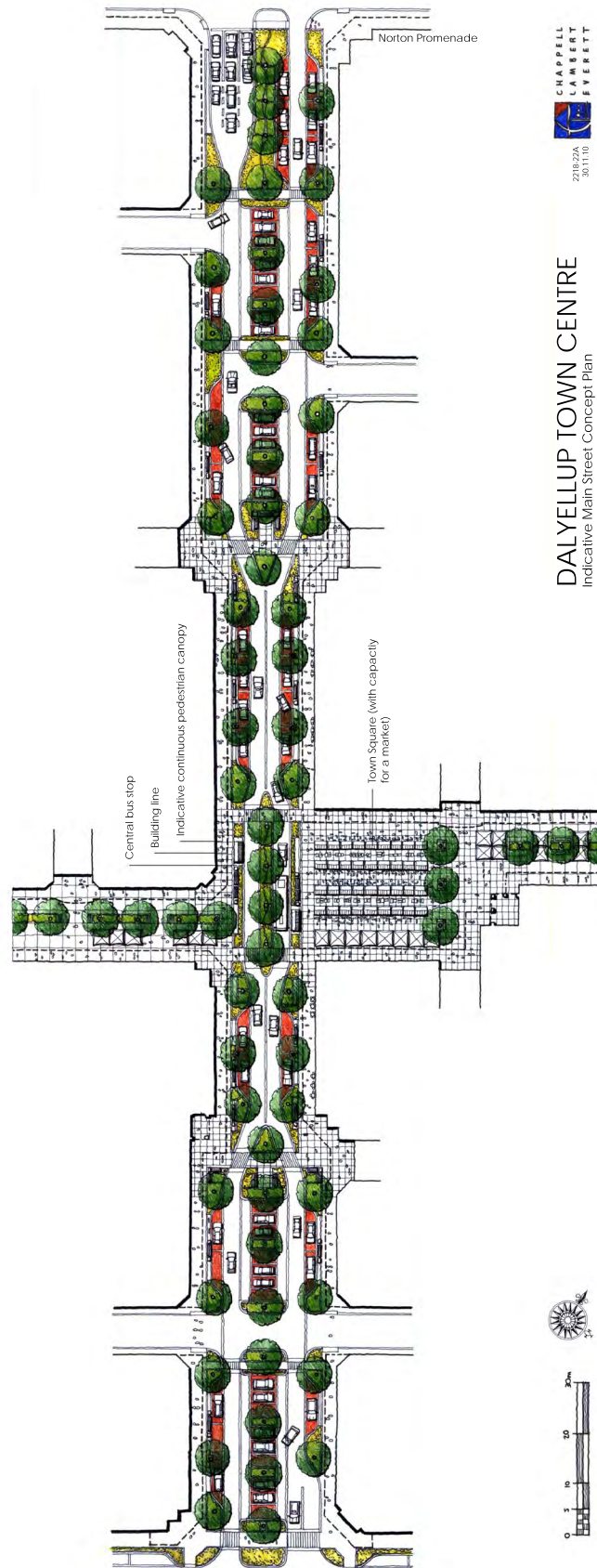


Figure 2A- Main Street Concept Plan

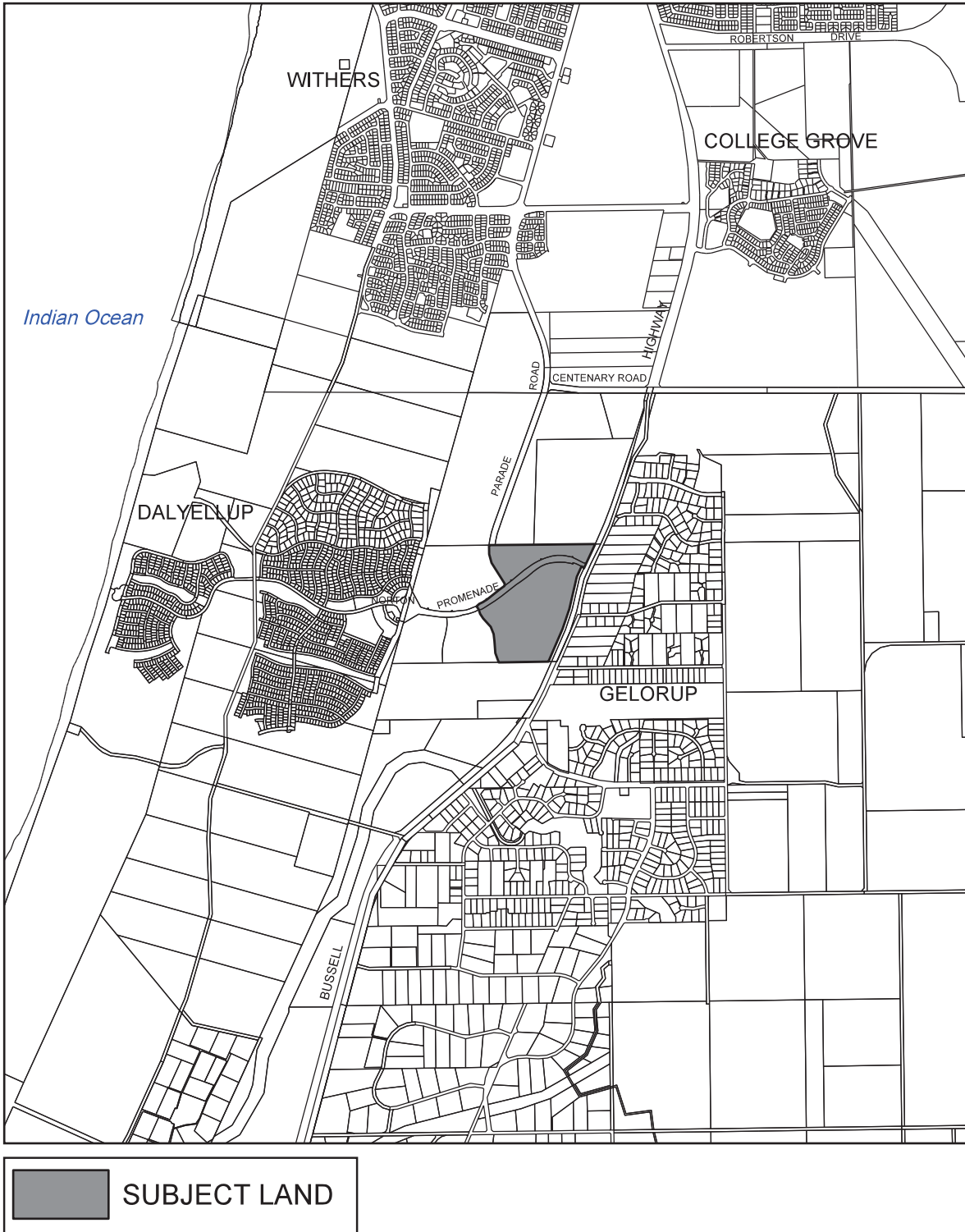


Figure 3 - Location Plan

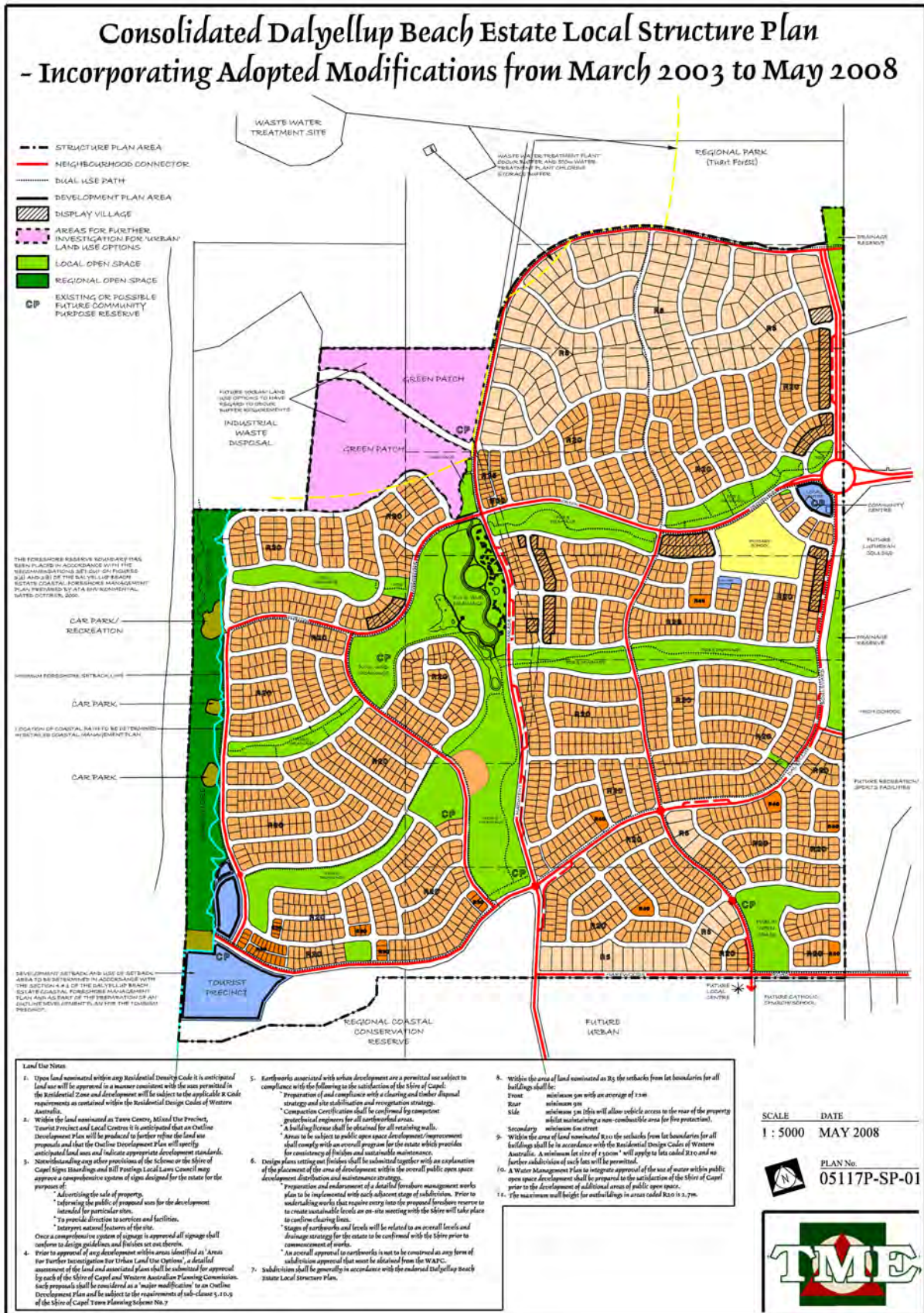


Figure 4 - Dalyellup Beach Estate Local Structure Plan

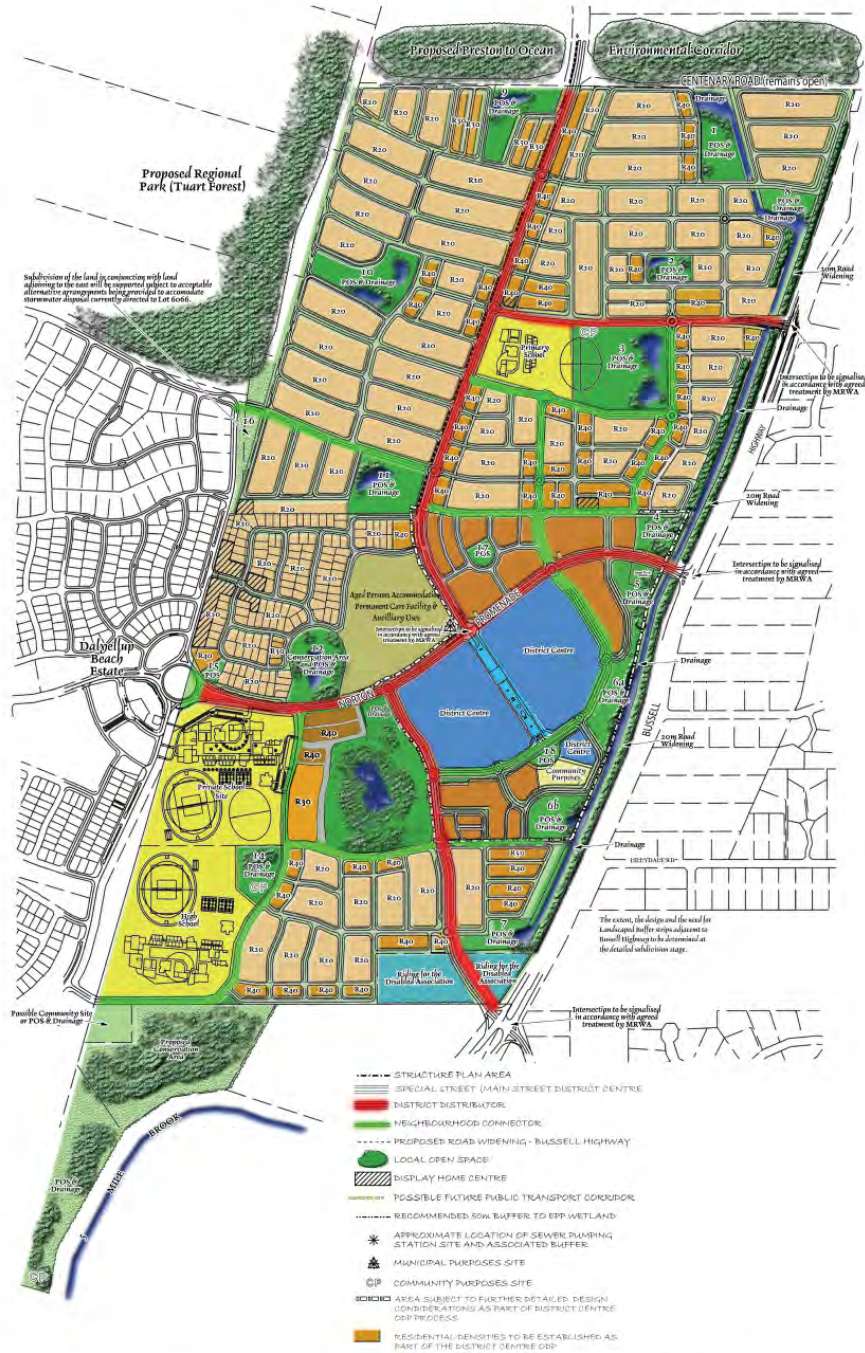


STRUCTURE PLAN DISCLAIMER: THIS STRUCTURE PLAN PROVIDES FRAMEWORK FOR MORE DETAILED PLANNING TO OCCUR AT THE SUBDIVISION AND DEVELOPMENT STAGE. THIS STRUCTURE PLAN IS TO BE READ IN CONJUNCTION WITH THE ACCOMPANYING REPORT.

Dalyellup East - Local Structure Plan

Consolidation March 2012

FOR ADDITIONAL NOTES SEE:
ADDENDUM 1 - LAND USE AND IMPLEMENTATION NOTES



| | | |
|---|--|---|
|  | SCALE DATE A1 - 1 : 5000 A3 - 1 : 10000 | CONSOLIDATION PLAN - MARCH 2012 Adopted pursuant to Delegation 210 |
| | PLAN No. | |

THOMPSON McROBERT ENGINEERS 26 Whitson Street, Bunbury WA 6230 Telephone: (08)9791 4411 Email: tme@burby@tme.net.au

Figure 5 - Dalyellup East Local Structure Plan



Precinct A: Retail Core

A main street setting incorporating a mix of uses including a supermarket, speciality retailing and entertainment

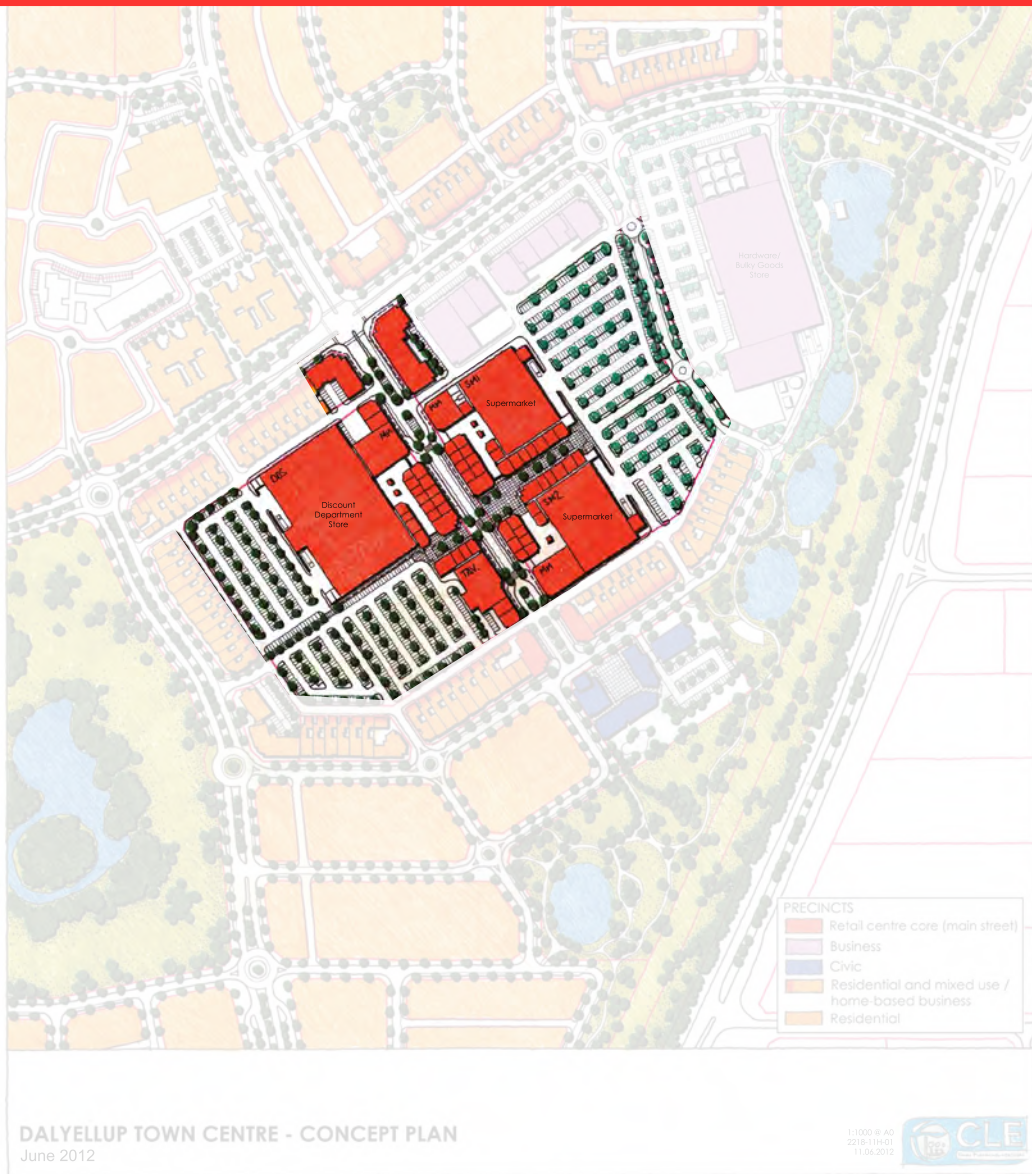


Figure 6 - Precinct A (Retail Core)



Attractive landscaped main street setting



A shady and vibrant town square to encourage activity and vitality

Entertainment areas mingling into the street



On street parking framing and providing a buffer to the pedestrian environment

Carparking softened by landscaping



Use of various materials, colours, textures and architectural elements to create visual interest

Shade structures protecting the pedestrian environment and encouraging public interaction



Opening up shopfronts to the street and extending the trading envelope.

A prominent landmark building announcing "arrival" to the town's heart



Buildings providing an attractive frame for a mix of uses to interact

Dalyellup District Centre
Outline Development Plan

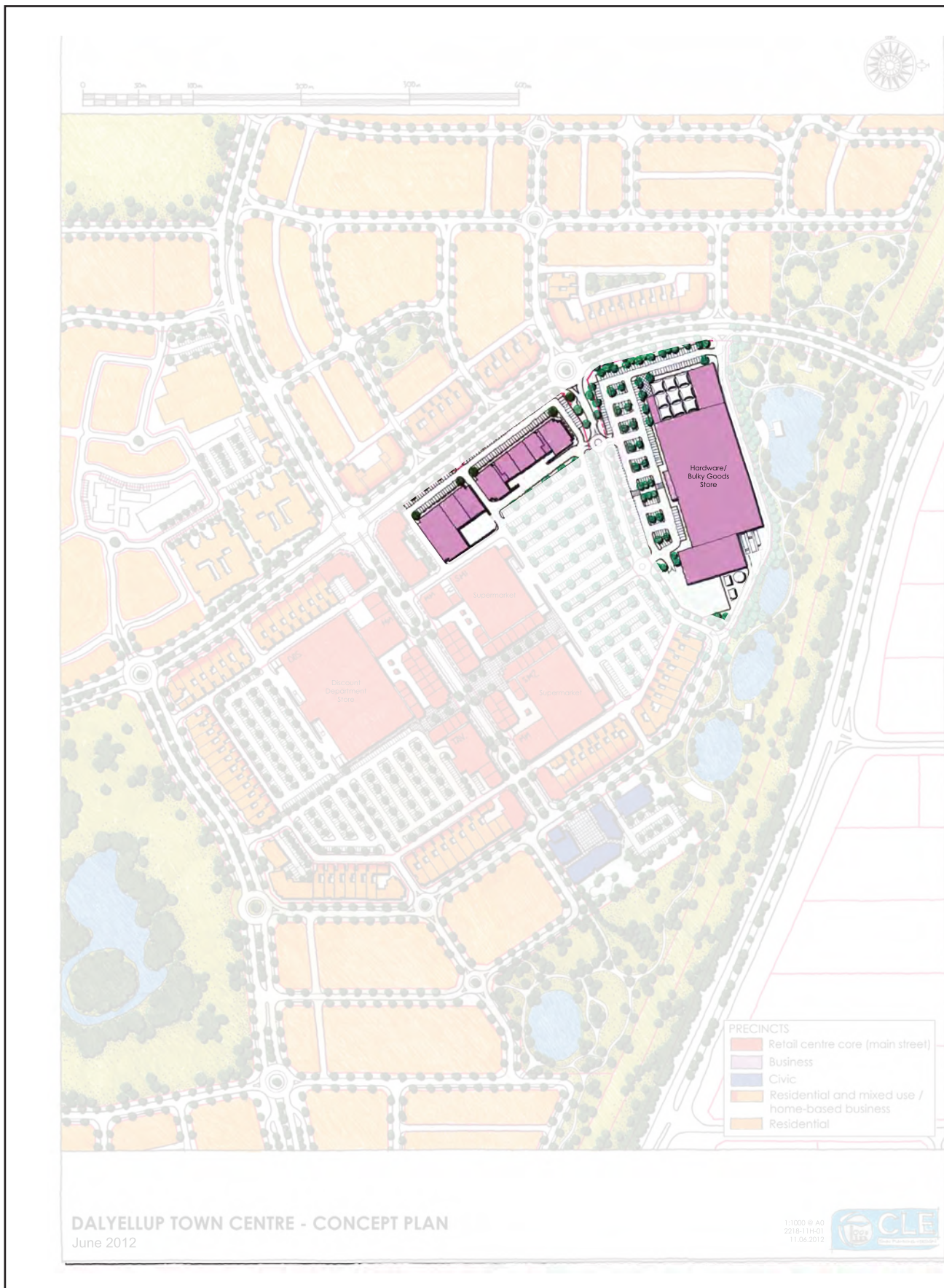


Figure 7 - Precinct B (Business)



Precinct B: Business

Building Design adding to street appeal and creating an attractive commercial gateway to the District Centre

Service commercial and other businesses requiring direct parking



Use of large areas of glazed surface to encourage openness and interaction



Use of colour and vertical lines to break built form bulk



Attractive office buildings that address the public environment



Dalyellup District Centre
Outline Development Plan



Figure 8 - Precinct C (Civic)



Precinct C: Civic

Modern landmark building design that positively encourages interaction with open park land setting

A modern landmark building that encourages public interaction and openness through use of glazed facades



A well shaded and landscaped pedestrian environment.

The selection of landscaping plants provides shade, yet retains an open aspect reducing opportunities for anti-social activities



Attractive landscaped park that is framed by buildings



Use of various materials, colours and textures as a visual language to define a landmark building character

Dalyellup District Centre
Outline Development Plan

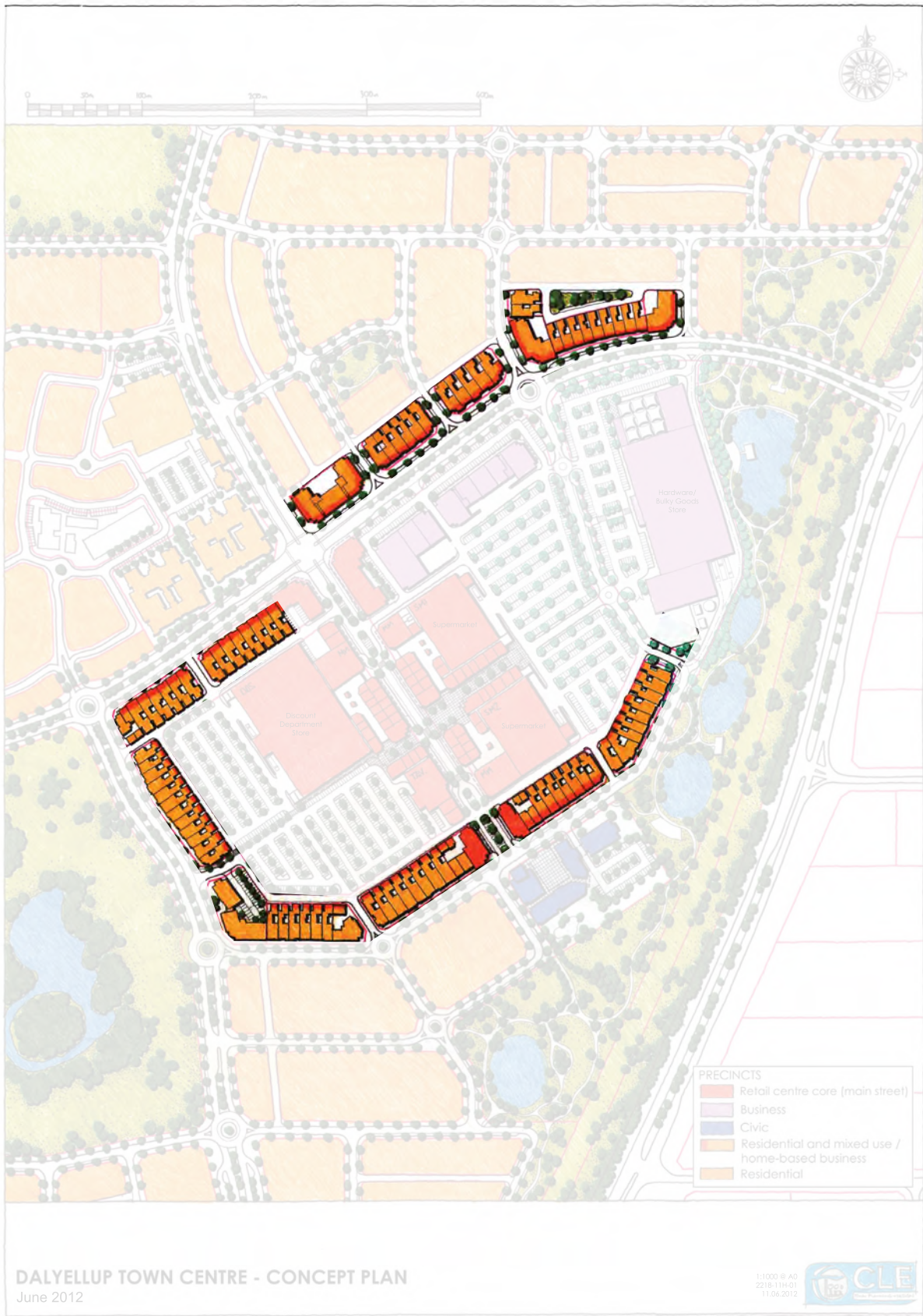


Figure 9 - Precinct D (Residential and Mixed Use)



Precinct D: Residential and Mixed Use
Medium density residential and mixed use buildings framing the edge of the retail core

Buildings overlooking park areas.



Attractive setting for small scale commercial activity at rear of residential lots



Landscaping defining use and providing shade



Use of low open fencing to define but encourage interaction with public/private realms



Ground floor commercial use with residential above



Rear laneways with building design creating opportunity for passive surveillance



On street visitor parking



Minimal setbacks and use of balconies to create a feeling of public safety

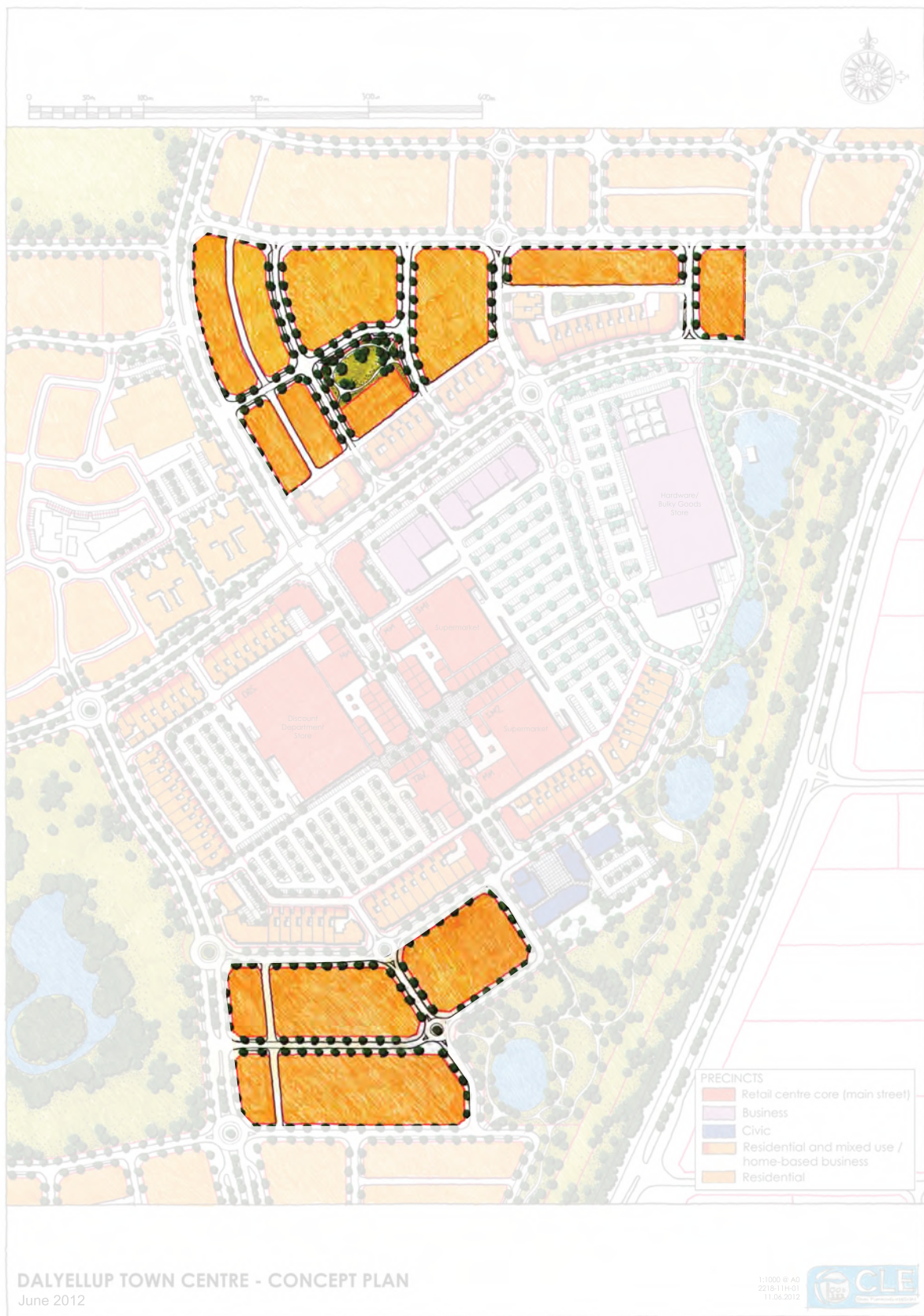


Figure 10 - Precinct E (Residential)



Precinct E: Residential

An attractive residential built form that compliments the scale and density of surrounding development

Houses incorporating architectural features and a variety of materials to create visual interest



Low rise apartments and townhouses with neighbourhood character

Opportunities for pathways to connect to linear park system



Onstreet visitor parking.

Houses featuring attractive pathway networks



Landscaping defining use and providing shade.



Figure 11 - Artist Impression of Main Street



APPENDIX 1

Dalyellup District Centre: Economic Report and Impact Statement



APPENDIX 2

Dalyellup District Centre: Traffic and Transport Report



APPENDIX 2A

Dalyellup District Centre: Traffic and Transport Addendum Report (March 2012)



APPENDIX 3

Servicing Strategy